

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	State Road (SR) 46/ Decatur County
<b>Designation Number(s):</b>	1800255 & 1800256
<b>Project Description/Termini:</b>	Pavement Rehabilitation/Designation Number (Des. No.) 1800255: SR 46 from SR 3 to the west junction with United States Highway (US) 421 and Des. No. 1800256: SR 46 from the SR 46 east junction with US 421 to Base Road.

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date
_____	
FHWA Signature and Date	

**Release for Public Involvement**

_____	_____
N/A	ADWP
INDOT DE Initials and Date	INDOT ESD Initials and Date
	March 29, 2023

**Certification of Public Involvement**

\_\_\_\_\_

INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:** \_\_\_\_\_

**Name and Organization of CE/EA Preparer:** \_\_\_\_\_

Harlan Ford/RQAW

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Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

## Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

### Notice of Entry Letters

Notice of Entry letters were mailed to potentially affected property owners near the project area on November 4, 2019, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, page G1.

### Section 106

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "Adverse Affect" was published online at: [Greensburg Daily News Public Notices](#) and in the *Greensburg Daily News* on July 26, 2022, offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed at least 30 days later, on August 30, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages D282 to D283. No comments were received.

### Public Informational Meeting

Invitation letters were mailed to all nearby property owners within the project area on April 29, 2022 (Appendix G, pages G2 to G3). A legal notice advertising the public informational meeting was published in the *Greensburg Daily News* newspaper on May 03, 2022 (Appendix G, pages G4 to G5). Project plans and a pre-recorded PowerPoint presentation was made available for review on the Seymour District website ([Seymour.indot.in.gov](#)) for anyone that would prefer or would be unable to attend. The PowerPoint presentation was also made available on INDOT's YouTube page at: [Public Information Des 1800255 & 1800256 SR 46 Greensburg](#). Additionally, INDOT Seymour District sent out a news release on May 3, 2022 and posted information about the upcoming public informational meeting on their social media pages on May 4, 2022, which provided contact information for any questions or comments. (Appendix G, page G6 to G7). The public informational meeting was held on May 10, 2022 at the Greensburg City Hall, 314 W. Washington Street, Greensburg, IN 47240. The meeting described the project scope and schedule through the use of a PowerPoint presentation and solicited input and comments from the community. Please refer to Appendix G, pages G9 to G14 for the PowerPoint presentation presented and Appendix G, pages G15 to G17 for graphics presented during the informational meeting. Additionally, please refer to Appendix G, pages G18 to G21 for the public sign in sheets from attendees. Verbal questions from the public pertained to the project design and consisted mostly of general project scope questions. However, some community members expressed their disinterest with the on-street parking and drainage system proposed; both of which, seemed to be lessened towards the end of the informational meeting. No comments were received prior to, or after the informational meeting was held.

### Public Hearing

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. However, the project sponsor as elected to forgo the opportunity to request a hearing and proceed with holding a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

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Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Seymour

Local Name of the Facility: SR 46

Funding Source (mark all that apply): Federal [X] State [X] Local [X] Other\* [ ]

\*If other is selected, please identify the funding source:

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The need for this project is based on the existing pavement deterioration, insufficient drainage, and lack of Americans with Disabilities Act (ADA) and Public Right of Way Accessibility Guidelines (PROWAG) compliant pedestrian facilities. The pavement failure is exhibited by transverse cracking, rutting, wheel path fatigue cracking, previous patching, and isolated areas with potholes as identified in the approved Engineers Report, dated May 2019 (Appendix I, pages I9 to I33). In addition, water regularly ponds along the project area and contributes to the poor pavement conditions.

Purpose: The purpose of this project is to add service life to the pavement (9-20 years minimum for Des No. 1800255, and 20 years minimum for Des No. 1800256) by improving the overall pavement condition and addressing drainage issues. In addition, this project seeks to enhance pedestrian facilities to ensure facilities meet current ADA and PROWAG standards and improve connectivity for residents within the community.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Decatur Municipality: SR 46/City of Greensburg

Limits of Proposed Work: Des No. 1800255 is from SR 3 to the west junction with United States Highway (US) 421, totaling approximately 1.8 miles in length. The project limits of Des. No. 1800256 are from the east junction with US 421 to Base Road, totaling approximately 0.8 miles in length.

Total Work Length: 2.6 Mile(s) Total Work Area: 29 Acre(s)

Is an Interstate Access Document (IAD)1 required? If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability? 1If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Yes1 No X Date:

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

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The Indiana Department of Transportation (INDOT), Seymour District with funding from the Federal Highway Administration (FHWA) intend to proceed with a pavement rehabilitation project.

### **Location:**

The project is located on SR 46 and crosses through the City of Greensburg in Decatur County Indiana. This project is divided into two sections with two separate Des No's., Des. No. 1800255 and Des. No. 1800256. Des No. 1800255 encompasses the western portion of the project area, while Des No. 1800256 encompasses the eastern portion of the project area. The project limits for Des No. 1800255 are from SR 3 to the west junction with United States Highway (US) 421, totaling approximately 1.8 miles in length. The project limits of Des. No. 1800256 are from the east junction with US 421 to Base Road, totaling approximately 0.8 miles in length. This project can be further described as being in Washington Township within Sections 1, 2, 3, 9, 10, 11, 12 of Township 10 North and Range 9 East. This project is located within the Forest Hill and Greensburg U.S. Geological Survey (USGS) Quadrangles. Please see Appendix B, pages B1 to B14 for project location and aerial maps.

### **Existing Conditions:**

SR 46 is classified as a Principal Arterial roadway and is not part of the of the National Highway System (NHS) but the north and east legs of the intersection of the west junction of US 421 (Ireland Road) and SR 46 are part of the National Truck Network (NTN).

### **Des No. 1800255**

Throughout the western project limits (Des No. 1800255), from SR 3 to Sunset Drive, SR 46 has a posted speed limit of 45 mph, which is reduced to 30 mph east of the Sunset Drive intersection. SR 46 consists mainly of two 12-foot wide travel lanes with paved shoulders varying in width from 8-feet west of the Central Railroad of Indiana railroad tracks, to 4-feet east of the railroad tracks. From N. West St. to the eastern project limits at the west junction US 421, SR 46 is predominately in a residential area. The roadway widens to a 3-lane section (two travel lanes, one in each direction and a turn lane) with a parking lane on the north side (46-foot total width) approaching the west junction US 421 intersection. Curbs are present from the Anderson Street (St.) intersection to the west junction US 421; the curb has experienced major deterioration and exhibits minimal curb exposure due to prior resurfacing projects. Curb is not present from the N. West St. intersection to the Anderson St. intersection, which allows stormwater runoff to flow directly onto the residential properties on the southside of the roadway. Minor pavement distresses are exhibited throughout this section of roadway such as, reflective cracking and fatigue cracking. Most private drive approaches consist of concrete and asphalt. Some of these locations east of N. West St. have significant surface discontinuities and slope deflections and will require improvement for pedestrian accessibility.

### **Drainage (Des No. 1800255)**

Along the segment of SR 46 from the western limits to a point approximately 0.5 mile east, drainage is conveyed via shallow roadside ditches and swales to a 24-inch culvert crossing under SR 46, which flows southwest to the Greensburg City Park Lake. From this point to the railroad tracks, drainage is conveyed via shallow roadside ditches and swales to Muddy Fork of Sand Creek, approximately 0.54 mile east of SR 3. SR 46 is carried over Muddy Fork of Sand Creek via a 60-foot span, reinforced concrete arch bridge, which flows southwest to the Greensburg City Park Lake. Within this area, Decatur County installed a storm sewer system consisting of area inlets located off the pavement from near the Sunset Drive intersection, west to Muddy Fork. Along the segment of SR 46 from the railroad tracks to N. West St, drainage is conveyed via shallow roadside ditches and grassy swales to a drainage ditch just east of the railroad crossing, which flows northwest along the railroad to an ephemeral stream near the railroad crossing along SR 3. Within this area, the City of Greensburg has noted that water regularly ponds along the south side of SR 46, likely contributing to the poor pavement condition.

From N. West St. to the west junction with US 421 (N. Ireland St.), drainage is conveyed with curbs along the edges of SR 46 and within a closed storm sewer system. The inlets west of N. Carver St. appear to be in a low-lying area along SR 46, with drainage outletting to the south, under a riprapped area on the west property line of the commercial strip development. The inlets at the Anderson St. intersection also appear to be in a low-lying area along SR 46. The outlet location for this low point is assumed to be to the south along Anderson St. Within this area, the City of Greensburg has noted that water regularly ponds in the Anderson St. intersection.

### **Sidewalks (Des No. 1800255)**

Sidewalks are present along the north side of SR 46 from the commercial strip development located 300 feet west of N. West Street and continue to the eastern project limits at the west junction with US 421 (N. Ireland St). The existing sidewalk is concrete and the width of sidewalk varies from 4 to 6 feet wide and is generally in poor to fair condition throughout the project limits. It is important to note that just east of the Tree County Playhouse, roughly 54 linear feet of the existing sidewalk consists of brick. Additionally, along the south side of SR 46 from the residential properties located 150 feet west of N. West Street and continue to the eastern project limits at the West Junction with US 421 (N. Ireland St). The existing sidewalk is concrete, and the width of sidewalk is typically 5 feet wide and is generally in poor to fair condition throughout the project limits (Appendix I, page I16 to I17). Additionally, all curb ramps are out of compliance with ADA and PROWAG guidelines for cross slope, running slope, and/or the presence of corrugations in the transitional areas (Appendix I, page I15).

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### **Des No. 1800256**

Throughout the eastern project limits (Des No. 1800256), SR 46 is predominately in a residential area and runs west-east with a posted speed limit of 30 mph. From the western project limits to Wilder St., the total paved width of SR 46 varies from 40 to 47.5 feet bounded by concrete curb on both sides, and consists of 1-travel lane in each direction, an eastbound left turn lane and westbound right turn lane at the N. Lincoln St. intersection, and parking lanes east of N. Lincoln St. From Wilder St. to a point approximately 500 feet west of Base Road, SR 46 consists mainly of two 11-foot travel lanes with a 7-foot parking lane bounded by concrete curb on both sides (36-foot total pavement width); east of this location, SR 46 consists of 12-foot travel lanes with 6-foot paved shoulders. All existing curb within the project limits has experienced major deterioration and exhibits minimal curb exposure due to prior resurfacing projects. Most private approaches consist of concrete and asphalt. Some locations have significant surface discontinuities and slope deflections and will require improvement for pedestrian accessibility.

### **Drainage (Des No. 1800256)**

Throughout the majority of the project limits, drainage is conveyed with curbs along the edges of SR 46 and within a closed storm sewer system, out-letting at Gas Creek, approximately 630 feet east of US 421; there is a structure carrying SR 46 over Gas Creek which was replaced as a separate project with a flat top, three-sided structure under Des No 1400150. Within this area, the City of Greensburg has noted that water regularly ponds at the low point near the Gas Creek crossing. The City also believes the inlet spacing and/or pipe capacity of the entire storm sewer system may be inadequate to handle storm water runoff in the area.

### **Sidewalks (Des No. 1800256)**

Sidewalks are present along the north side of SR 46 from the western project limits and continue to the Greensburg Public Library, located west of E. Washington St., and connect to the library entrance. The existing sidewalk is concrete and the width of sidewalk varies from 5 to 6 feet wide and is generally in poor to fair condition throughout the project limits. Additionally, sidewalks are present along the south side of SR 46 from the western project limits and continue to the auto parts store, 250 feet east of N. Davidson St. The existing sidewalk is concrete and the width of sidewalk varies from 5 to 6 feet wide and is generally in poor to fair condition throughout the project limits (Appendix I, pages 117 to 119). Additionally, all curb ramps are out of compliance with ADA and PROWAG guidelines for cross slope, running slope, and/or the presence of corrugations in the transitional areas (Appendix I, page 115).

### **Preferred Alternative:**

The preferred alternative within the limits of Des No. 1800255 consists of the following:

- From the SR 3 intersection to the CIND railroad crossing the proposed project recommends milling and applying a Hot Mix Asphalt (HMA) preventative maintenance overlay (including shoulders), with partial and full depth pavement patching at various locations. No drainage improvements are anticipated in this section.
- From the CIND railroad crossing to the west junction with US 421, the proposed project recommends full depth pavement replacement along with replacement and/or construction of a stormwater sewer system, sanitary sewer system, water main installation, and curb and gutter. In addition, new street lighting will be installed along this portion of the project.
  - Stormwater Sewer System: All existing stormwater facilities will be removed and a new stormwater sewer will be constructed within the described limits. No existing facilities will remain.
  - Water Main Installation: Existing watermains and lines connecting to the mains will be replaced within right-of-way. Old water mains will be removed, and new water mains and lines will be put in their place in the same footprint.
  - Curb and Gutter: All existing curb and gutter within the above stated limits will be removed. New curb and gutter will be constructed with the new, full-depth roadway.
- Sidewalk construction/reconstruction will occur from the Needler's (736 W. Main St.) parking lot to the intersection of SR 46 and Ireland St. on both the north and south sides. In addition, from the Needler's parking lot, heading west to the CIND railroad crossing, there will be a sidewalk added to the north side only.
- The intersections of West Street, Carver Street, Anderson Street, and US 421 (Ireland St.) are proposed to be reconstructed to meet current Americans with Disabilities Act (ADA) and PROWAG standards.
  - West Street: Will replace existing curb ramps with ADA complaint curb ramps. Sidewalk at the intersection will be removed and replaced with new sidewalk, ramps (sloping sidewalk), and visually impaired detectable warning surfaces (red bump areas). Work will be within the existing sidewalk footprint.
  - Carver St.: Will replace existing curb ramps with ADA complaint curb ramps. Sidewalk at the intersection will be removed and replaced with new sidewalk, ramps (sloping sidewalk), and visually impaired detectable warning surfaces (red bump areas). Work will be within the existing sidewalk footprint.
  - Anderson St.: Will replace existing curb ramps with ADA complaint curb ramps. Sidewalk at the intersection will be removed and replaced with new sidewalk, ramps (sloping sidewalk), and visually impaired detectable warning surfaces (red bump areas). Work will be within the existing sidewalk footprint.
  - US 421: The project will include reconstructing the intersection at the west junction of US 421 and SR 46 (locally

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known as Main Street and Ireland Street). In addition to the pavement reconstruction at this location the scope of work includes new storm sewer and inlets, improved turning radii in the northeast quadrant (moving the new curb to roughly the back of the existing sidewalk), adjusting the signal pole locations, and replacing curb ramps to meet current ADA standards.

The preferred alternative within the limits of Des No. 1800256 consists of the following:

- Full depth pavement replacement, and reconstruction/construction of sidewalks along SR 46 from the east junction of US 421 to the drive to the Greensburg Public Library (approximately 0.09 mile west of the Base Road intersection).
- Along the north side of SR 46, it is proposed that the existing sidewalk will be replaced with a 8-10 foot-wide multi-use path. Additionally, the south side of SR 46 will have 5 feet wide sidewalks and a 5 feet wide grass buffer.
- The replacement and/or construction of a stormwater sewer system, water main installation, street lighting, and curb and gutter is proposed for this portion of the project.
  - Stormwater Sewer System: All existing stormwater facilities will be removed and a new stormwater sewer will be constructed within the described limits. No existing facilities will remain.
  - Water Main Installation: Existing watermains and lines connecting to the mains will be replaced within right-of-way. Old water mains will be removed and new water mains and lines will be put in their place in the same footprint.
  - Curb and Gutter: All existing curb and gutter within the stated limits will be removed. New curb and gutter will be constructed with the new, full-depth roadway.
  - Lighting: Lighting is not currently present along SR 46 in the project sections. New poles, luminaires, and wiring will be constructed along SR 46.
- The existing travel lane and parking configuration will change, but one travel lane in each direction will remain. On street parking areas along the south side of SR 46 will be from Lincoln St. to Vine St. and parking on the north side between Stewart St. and Davidson St.
- As part of the proposed sidewalk reconstruction, the intersection at Wilder St. will include "bump outs" at the west side of the intersection to improve pedestrian access and promote decreased traffic speed.
- As part of the proposed pavement replacement, the intersection at N. Lincoln St. will include 12-foot lane widths for both eastbound and westbound SR 46 travel lanes at the intersection (sidewalk buffer widths will be reduced to 5 feet to allow the wider lanes to be provided within the existing right-of-way) and the pavement markings for the eastbound left turn lane should be extended an additional 200 feet (currently 110 feet) to provide for some additional deceleration time. The traffic signals will also be replaced at this location.
- From the drive to the Greensburg Public Library to the eastern project termini (0.01 mile east of Base Road intersection), an HMA preventative maintenance overlay is anticipated.

Please note that the City of Greensburg will be funding all replacement and/or construction of a stormwater sewer system, water main installation, and street lighting. Please refer to the project plans in Appendix B, pages B30 to B149 for plan sheets that depict the above described work.

### **Maintenance of Traffic (MOT):**

The MOT plan for this project will consist of a road closure with a detour route. Please refer to the *MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION* section of this CE document for more details.

### **Logical Termini/Independent Utility:**

Des No. 1800255 begins at the SR 3/SR 46 intersection and extends to the intersection at the west junction of US 421 and SR 46 (locally known as Main Street and Ireland Street). The termini are logical as it begins at a major intersection and ends at the west junction of US 421 and SR 46 intersection, where there was a previously completed pavement improvement project along US 421 (segment of US 421 in between Des No's 1800255 and 1800256), and the pavement is in sound condition. There is currently a separate planned intersection improvement project at the SR 3/SR 46 intersection where the project begins, but as this project consists of an HMA overlay in this area it will not restrict the alternatives of any reasonably foreseeable transportation improvement projects. Furthermore, Des No. 1800255 demonstrates independent utility as it can function as a standalone project and is not dependent on any other projects.

Des No. 1800256 extends from the intersection at the east junction of SR 46 and US 421 (locally known as Main Street and S. Michigan Ave.) to 0.01 mile east of the base road intersection. The termini are logical as it starts where there was a previously completed project along US 421 (as mentioned above) where the pavement is in sound condition and ends where the project area starts to transition from an urban to rural setting. In addition, the pavement condition in the area surrounding base road is in sound condition. Please note that the multi-use path proposed to be installed as part of this project terminates just east of the Greensburg Public Library (approximately 0.09 mile west of the Base Road intersection). A turn around will be installed where the multi-use path terminates, but there are future plans to connect this multi-use path to the nearby Rebekah Park as outlined in the Greensburg Bicycle and Pedestrian System Plan (Appendix I, page I34 to I35). Des No. 1800256 demonstrates independent utility as it can

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function as a standalone project and is not dependent on any other projects.

This project meets the purpose and need of the project through a combination of HMA overlay and full depth HMA pavement replacement, which improves and increases the overall service life of the pavement. In addition, this project satisfies the purpose and need by improving the drainage through replacement/construction of a stormwater sewer system (including inlets) and by adding curb and gutter to direct the flow of stormwater and prevent water ponding. Lastly, the project meets the purpose and need through reconstruction of curb ramps to meet current ADA and PROWAG standards and replacing the existing sidewalk with a multi-use path to improve connectivity for residents along the north side of SR 46 from Lincoln St to the E. Washington St./ Base Road approach with an 8-10 foot wide multi-use path.

**OTHER ALTERNATIVES CONSIDERED:**

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

**No Build Alternative:**

This alternative would not involve any improvements to the existing roadway, pedestrian facilities, or address any of the drainage issues present within the project area. Although this alternative would not require additional right-of-way, would not involve any cost, and would not result in any impacts to the surrounding environment, it was ultimately dismissed as it would not meet the purpose and need of the project.

**Alternate for Des No. 1800255 (From CIND Railroad Crossing to West Junction with US 421)**

This alternative proposed Portland Cement Concrete Pavement (PCCP) compared to the preferred alternative of Full depth HMA pavement replacement along this section of SR 46. This alternative would have similar environmental impacts but a higher initial cost than the preferred alternative. Comparing the pavement costs per lane-mile, the preferred alternative is \$227,000 and this alternative is \$307,500, when factoring in the longer design life of this alternative (30 years vs. 20 years), the per lane-mile per year cost of this alternative is slightly less than that of the preferred alternative (\$10,300 vs. \$11,400). Although this alternative would meet the purpose and need of the project, due to the preferred alternative having a lower initial cost coupled with maintaining a consistent pavement type throughout the corridor, this alternative was ultimately dismissed.

**Alternate for Des No. 1800256 (From East Junction of US 421 to just east of the Greensburg Library)**

This alternative proposed Portland Cement Concrete Pavement (PCCP) compared to the preferred alternative of Full depth HMA pavement replacement along this section of SR 46. This alternative would have similar environmental impacts but a higher initial cost than the preferred alternative. Comparing the pavement costs per lane-mile, the preferred alternative is \$715,500 and this alternate is \$967,500, when factoring in the longer design life of this alternate (30 years vs. 20 years), the per lane-mile per year cost of this alternate is slightly less than preferred alternative (\$32,300 vs. \$35,800). Although this alternative would meet the purpose and need of the project, due to the preferred alternative having a lower initial cost coupled with maintaining a consistent pavement type throughout the corridor, this alternative was ultimately dismissed.

**The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply):**

- It would not correct existing capacity deficiencies;
  - It would not correct existing safety hazards;
  - It would not correct the existing roadway geometric deficiencies;
  - It would not correct existing deteriorated conditions and maintenance problems; or
  - It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

X

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### ROADWAY CHARACTER:

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway SR 46  
 Functional Classification: Urban Principal Arterial  
 Current ADT: 4,773-10,573 VPD (2017) Design Year ADT: 6,380-14,120 VPD (2043)  
 Design Hour Volume (DHV): 445-939 Truck Percentage (%) 10  
 Designed Speed (mph): 30 Legal Speed (mph): 30

	Existing		Proposed
Number of Lanes:	2-3		2-3
Type of Lanes:	2 travel and 1 parking		2 travel and 1 parking
Pavement Width:	46	ft.	40-47.5
Shoulder Width:	4-8	ft.	4-8
Median Width:	N/A	ft.	N/A
Sidewalk Width:	4-8	ft.	5-10

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

Name of Roadway US 421  
 Functional Classification: Other Principal Arterial  
 Current ADT: 7,206 VPD (2022) Design Year ADT: 8,215 VPD (2044)  
 Design Hour Volume (DHV): 1,216 Truck Percentage (%) 11  
 Designed Speed (mph): 45 Legal Speed (mph): 30-45mph

	Existing		Proposed
Number of Lanes:	2		2
Type of Lanes:	Travel		Travel
Pavement Width:	42	ft.	42
Shoulder Width:	N/A	ft.	N/A
Median Width:	N/A	ft.	N/A
Sidewalk Width:	5-7	ft.	5-7

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

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### BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): 046-16-00945/017400 Sufficiency Rating: 97.0 (INDOT, BIAS)  
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	Concrete Culvert		N/A	
Number of Spans:	1		N/A	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	44	ft.	N/A	ft.
Outside to Outside Width:	49	ft.	N/A	ft.
Shoulder Width:	4	ft.	N/A	ft.

Structure/NBI Number(s): 046-16-10454/017407 Sufficiency Rating: 99.3 (INDOT, BIAS)  
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	Concrete Culvert		N/A	
Number of Spans:	1		N/A	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	45.3	ft.	N/A	ft.
Outside to Outside Width:	84	ft.	N/A	ft.
Shoulder Width:	N/A	ft.	N/A	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

There are several small structures that do not have assigned structure No's and two bridges (Bridge No's. 046-16-00945 and 046-016-10454) that exist within the project area. However, this project does not include work on any bridges and/or small structures such as culverts/pipes. Please note that storm sewer structures will be replaced as part of this project, but they are enclosed structures that do not allow for bats or birds to inhabit them.

### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?		X
Will the project involve the use of a detour or require a ramp closure? (describe below)	X	
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	X	
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	X	

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

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**Des No. 1800255:**

The MOT plan for this project will consist of a full road closure with an official detour route. The official detour route will utilize US 421 and SR 3. The official detour route will be approximately 2.58 miles in length. This will add roughly 0.82 mile when compared to the straight-line distance of SR 46, which is approximately 1.76 miles. Please refer to the plan sheets in Appendix B, page B41, which details the MOT plan.

**Des No. 1800256:**

The MOT plan for this project will consist of a full road closure with a detour route. The detour route will utilize I-74 (New Point Exit), SR 3, and US 421. The detour route will be approximately 22.44 miles in length. This will add roughly 21.64 miles when compared to the straight-line distance of SR 46, which is approximately 0.8 mile in length. Please refer to the plan sheets in Appendix B, pages B108 to B109, which details the MOT plan.

Please note that a Unique Special Provision (USP) will be developed for the contractor to notify all emergency and school officials prior to any construction that would limit or block access. An additional USP will be developed for the contractor to maintain access to all properties at all times during construction. A firm commitment to incorporate both USP's have been added to the *Environmental Commitments* section of this CE document. School bus routes will likely be impacted by this project, however; school buses will likely utilize local roads as detours during construction. Unofficial local detours may be implemented by the City or County for local traffic at their discretion. Local truck traffic may use Washington Street, which parallels SR 46, but in an unofficial capacity. To date, no agreement between the City, County, or INDOT exists for an unofficial detour route.

The closures will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

In addition, to complete the work proposed on the sidewalks and ADA curb ramps, pedestrians will be required to use a detour. The pedestrian MOT will consist of temporary curb ramps located behind temporary traffic barriers. Signage and temporary pavement markings (crosswalks) will be provided along the detour route to direct pedestrians where to cross. Please refer to Appendix B, pages B42 to B43 and B110 to B111 for plan sheets that illustrates the pedestrian MOT.

**ESTIMATED PROJECT COST AND SCHEDULE:**

Engineering: \$ 1,000,000 (2019) Right-of-Way: \$ 400,000 (2022) Construction: \$ 9,011,365 (2024)

Anticipated Start Date of Construction: Spring 2024

**RIGHT OF WAY:**

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.19	0.85
Commercial	0.63	0.24
Agricultural	0	0
Forest	0	0
Wetlands	0	0.01
Other:	0	0
Other:	0	0
<b>TOTAL</b>	<b>0.82</b>	<b>1.1</b>

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

The apparent existing right-of-way (ROW) generally varies through the project area 0-15 feet from the edge of pavement and/or the back of existing sidewalk (0-15 feet). Existing ROW is used for maintenance of SR 46 and consists primarily of the roadway itself and

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existing sidewalks.

Des No. 1800255 will require approximately 0.56 acre of new permanent ROW from both residential and commercial properties. Additionally, 0.31 acre of temporary ROW will be needed from both residential and commercial properties and Wetland E.

Des No. 1800256 will require approximately 0.26 acre of new permanent ROW from residential and commercial properties. Additionally, 0.80 acre of temporary ROW will be needed from residential and commercial properties for drive reconstruction and curb ramp work.

Cumulatively, this project will require 0.82 acres of permanent ROW and 1.1 acre of temporary ROW.

If the scope of work or permanent or temporary right-of-way amounts change, INDOT ESD and the INDOT District Environmental Section will be contacted immediately.

### Part III – Identification and Evaluation of Impacts of the Proposed Action

#### **SECTION A - EARLY COORDINATION:**

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early Coordination Letters were sent out to agencies on September 13, 2021 (Appendix C, pages C1 to C4).

Agency	Date Sent	Date Response Received	Appendix
INDOT, Seymour District	September 13, 2021	No Response Received	N/A
Federal Highway Administration (FHWA)	September 13, 2021	No Response Received	N/A
Natural Resources Conservation (NRCS)	September 13, 2021 <sup>1</sup> October 01, 2021	September 15, 2021 October 14, 2021	C21 to C22
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife	September 13, 2021 <sup>1</sup> April 04, 2022	October 13, 2021 April 6, 2022	C23 to C27
U.S. Department of Housing and Urban Development (USHUD)	September 13, 2021	No Response Received	N/A
United States Fish and Wildlife Service (USFWS)	September 13, 2021	September 27, 2021	C28 to C29
United States Army Corps of Engineers (USACE)	September 13, 2021	No Response Received	N/A
INDOT, Aviation	September 13, 2021 <sup>1</sup> October 01, 2021 <sup>1</sup> October 04, 2021	September 15, 2021 October 04, 2021 October 05, 2021	C16 to C19
Municipal Separate Storm Sewer System (MS4) Coordinator, City of Greensburg	September 13, 2021	No Response Received	N/A
National Park Service (NPS)	September 13, 2021	No Response Received	N/A
Decatur County Surveyor	September 13, 2021	No Response Received	N/A
City of Greensburg Mayor	September 13, 2021	No Response Received	N/A
City of Greensburg Plan Commission/City Council	September 13, 2021	No Response Received	N/A
Greensburg Board of Works	September 13, 2021	No Response Received	N/A
Greensburg Board of Water	September 13, 2021	No Response Received	N/A
Greensburg Board of Aviation	September 13, 2021	No Response Received	N/A
Decatur County Area Plan Commission	September 13, 2021	No Response Received	N/A
Decatur County Highway Department	September 13, 2021	No Response Received	N/A
Decatur County Soil and Water Conservation	September 13, 2021	No Response Received	N/A
Decatur County Council	September 13, 2021	No Response Received	N/A

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Indiana Department of Environmental Management (IDEM) automated response	September 13, 2021	September 13, 2021	C5 to C11
IDEM, Groundwater	September 13, 2021	September 17, 2021	C12
City of Greensburg Engineer/ Local Floodplain Administrator	September 13, 2021	No Response Received	N/A
IDNR, Division of Oil and Gas	September 13, 2021	October 19, 2021	C20
Indiana Geological and Water Survey (IGWS)	September 13, 2021	September 13, 2021	C13 to C15
Decatur County Parks and Recreation	September 13, 2021	No Response Received	N/A
Greensburg Country Club	September 13, 2021	No Response Received	N/A
Greensburg Fire Department	September 13, 2021	No Response Received	N/A
Greensburg Police Department	September 13, 2021	No Response Received	N/A
Decatur County Memorial Hospital	September 13, 2021	No Response Received	N/A
Decatur County Sheriffs Department	September 13, 2021	No Response Received	N/A
Saint Mary's School	September 13, 2021	No Response Received	N/A
Good Shepherd Christian Academy	September 13, 2021	No Response Received	N/A
<sup>2</sup> Billings Elementary School	September 13, 2021	No Response Received	N/A
Greensburg Junior High School	September 13, 2021	No Response Received	N/A
Greensburg High School	September 13, 2021	No Response Received	N/A
Greensburg Community Learning Center	September 13, 2021	No Response Received	N/A
Greensburg Elementary School	September 13, 2021	No Response Received	N/A
<sup>3</sup> Westport Water Company	September 27, 2021	No Response Received	N/A

<sup>1</sup>Please note that the additional coordination dates with the NRCS, IDNR, and INDOT, Aviation represent continued coordination efforts based on their responses.

<sup>2</sup>Please note that a return to sender letter was received back from coordination with the Billings Elementary School located at 221 N. Monfort Street, Greensburg, IN 47240. Upon further review, it was determined that this school was closed and no longer in operation.

<sup>3</sup>Please note that coordination with the Westport Water company occurred on September 27, 2021 as a result of the early coordination response received from IDEM's Groundwater section (Appendix C, page C12). No response was received from the Westport Water Company.

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

### SECTION B – ECOLOGICAL RESOURCES:

#### Streams, Rivers, Watercourses & Other Jurisdictional Features

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

#### Presence

X

#### Impacts

Yes	No
	X

Total stream(s) in project area: 465 Linear feet      Total impacted stream(s): 0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
UNT 1 to Muddy Fork Sand Creek	Intermittent	95	0	Lat: 39.33415° Long: -85.51508°, flows in a northwest to southeast direction, likely a <i>Waters of the U.S.</i> , (Appendix F, page F27)

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Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
Muddy Fork Sand Creek	Perennial	157	0	Lat: 39.33591° Long: -85.51215°, flows in a northeast to southwest direction, likely a <i>Waters of the U.S.</i> , (Appendix F, page F27)
UNT 1 to Sand Creek	Perennial	213	0	Lat: 39.33669° Long: -85.47920°. flows in a north to south direction, likely a <i>Waters of the U.S.</i> , (Appendix F, page F29)

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial maps of the project area (Appendix B, pages B3 to B14), and the RFI report (Appendix E, pages E1 to E17) there are 20 streams, rivers, watercourse or other jurisdictional features within the 0.5-mile search radius. There are three rivers, watercourse or other jurisdictional features within or adjacent to the project area. That number was confirmed by the site visit on June 09, 2021 by RQAW.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on October 21, 2021. Please refer to Appendix F, pages F1 to F34 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that three likely jurisdictional streams exist within the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

### **UNT 1 to Muddy Fork Sand Creek**

This stream flows in a northwest to southeast direction and originates at an inlet on the northwest side of SR 46, which is within the western survey area (Des. No. 1800255). This stream was determined to have intermittent flow and exhibited an ordinary high water mark (OHWM) of 4.5 feet wide and 4 inches (0.33 feet) deep. This stream was determined to have poor quality due to roadway runoff and predominantly silt substrate. Approximately 95 linear feet (0.01 acre) of UNT 1 to Muddy Fork Sand Creek is within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, at the location of this stream the construction limits are confined to the pavements edge as shown on the project plan sheet in Appendix B, page B47. Therefore, impacts are not expected.

### **Muddy Fork Sand Creek**

This stream flows in a northeast to southwest direction through the western survey area (Des. No. 1800255). This stream was determined to have perennial flow and exhibited an OHWM of 31 feet wide and 18 inches (1.5 feet) deep. This stream was determined to be of average quality and has a substrate predominantly of sand and gravel. Approximately 157 linear feet (0.112 acre) of Muddy Fork Sand Creek is within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, at the location of this stream the construction limits are confined to the pavements edge as shown on the project plan sheet in Appendix B, page B48. Therefore, impacts are not expected.

### **UNT 1 to Sand Creek**

This stream flows in a north to south direction through the eastern survey area (Des. No. 1800256) and is locally known as Gas Creek. This stream was determined to have perennial flow and exhibited an OHWM of 11.2 feet wide and 7 inches (0.58 feet) deep. This stream was determined to have average quality and has a substrate of predominantly sand with riprap present at the structure. Approximately 213 linear feet (0.055 acre) of UNT 1 to Sand Creek is within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, there will be no impacts below the OHWM of UNT 1 to Sand Creek as shown in the project plan sheet in Appendix B, page B112. Therefore, impacts are not expected.

None of the above listed streams are listed as a State Natural and Scenic River, Outstanding River, nor are they listed as navigable waterways.

UNT 1 to Muddy Fork Sand Creek, Muddy Fork Sand Creek, and UNT 1 to Sand Creek will be labeled on the plans as "Do Not Disturb". This is included as a firm commitment in the *Environmental Commitments* section of this document.

### **Early Coordination**

The USFWS responded on September 20, 2021 stating that stream impacts may require permits from the USACE, IDEM Water Quality Certification (WQC) program, and the IDNR (Appendix C, pages C28 to C29). They went on to provide standard recommendations such as, restricting below low-water work in streams, restricting in channel work, avoid working in streams during

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fish spawning season, and implementing temporary erosion and sediment control measures.

The IDNR-DFW responded on October 13, 2021, providing general comments and standard recommendations pertaining to stream impacts, such as implementing erosion and sediment control measures, stream bank stabilization measures, minimizing in-channel disturbance, and time restrictions for working within the waterway (Appendix C, pages C23 to C25). They also went on to state that further coordination should occur once more information is available concerning the bridges over Muddy Fork Sand Creek, and UNT 1 to Sand Creek. RQAW followed up with IDNR-DFW on April 05, 2022 stating that no impacts to the aforementioned bridges or streams will occur as a result of this project. The IDNR-DFW responded on April 06, 2022 stating that they have no additional information to provide since there will be no bridge work or stream impacts (Appendix C, pages C26 to C27). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial maps of the project area (Appendix B, pages B3 to B14), and the RFI report (Appendix E, pages E1 to E17) there are seven open water resources within the 0.5 mile search radius. There is one open water feature adjacent to the south of the project area. That number was not confirmed, as no open water features are located within or adjacent to the project area as determined by the site visit on June 09, 2021. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on October 21, 2021. Please refer to Appendix F, pages F1 to F34 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no open water features exist within or adjacent to the project area.

**Early Coordination**

No early coordination responses were received concerning open water resources.

Wetlands	Presence	Impacts	
		Yes	No
	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.363 Acre(s) Total wetland area impacted: 0.01 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland A	PEM	0.041	0	Lat: 39.33354° Long: -85.52061°, likely <i>Waters of the U.S.</i> , Appendix F, page F27)
Wetland B	PEM	0.081	0	Lat: 39.33341° Long: -85.51832°, likely <i>Waters of the U.S.</i> , Appendix F, page F27)
Wetland C	PEM	0.188	0	Lat: 39.33409° Long: -85.51572°, likely <i>Waters of the U.S.</i> , Appendix F, page F27)
Wetland D	PEM	0.012	0	Lat: 39.33396° Long: -85.51548°, likely <i>Waters of the U.S.</i> , Appendix F, page F27)

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Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland E	PEM	0.041	0.01	Lat: 39.33659° Long: -85.49810°, likely Waters of the U.S., Appendix F, page F28)

	<u>Documentation</u>	<u>ESD Approval Dates</u>
<b>Wetlands</b> (Mark all that apply)		
Wetland Determination	<input checked="" type="checkbox"/>	<b>October 21, 2021</b>
Wetland Delineation	<input checked="" type="checkbox"/>	<b>October 21, 2021</b>
USACE Isolated Waters Determination	<input type="checkbox"/>	

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>
The project not meeting the identified needs.	<input checked="" type="checkbox"/>

*Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial maps of the project area (Appendix B, pages B3 to B14), and the RFI report (Appendix E, pages E1 to E17) there are 13 wetlands located within the 0.5 mile search radius. There is one wetland adjacent to the project area. However, during the site visit on June 09, 2021 by RQAW, a total of 5 wetlands were found within the project area.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on October 21, 2021. Please refer to Appendix F, pages F1 to F34 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that 5 likely jurisdictional wetlands exist within the project area. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

**Wetland A**

Wetland A is a palustrine emergent (PEM) wetland located within the western survey area (Des. No. 1800255) at the southeast quadrant of the SR 46 and SR 3 intersection. This wetland was determined to be of poor quality due its relatively small size, low species diversity, and disturbance from the roadway. Approximately 0.041 acre exists within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, Wetland A is located outside the construction limits for this project as shown on the project plan sheet in Appendix B, page B44. Therefore, impacts are not expected.

**Wetland B**

Wetland B is a PEM wetland located within the western survey area (Des. No. 1800255) at the SR 46 and S. 200 W. intersection. This wetland was considered to be poor quality primarily due to its small size, low species diversity, and disturbance from the roadway. Approximately 0.081 acre exists within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, Wetland B is located outside the construction limits for this project as shown in the project plan sheets in Appendix B, pages B45 and B46. Therefore, impacts are not expected.

**Wetland C**

Wetland C is a PEM wetland located within the western survey area (Des. No. 1800255) approximately 580 feet east of the SR 46 and S. 200 W. intersection. This wetland was considered to be poor quality primarily due to its small size, low species diversity, and disturbance from the roadway. Approximately 0.188 acre exists within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, Wetland C is located outside the construction limits for this project as shown in the project plan sheets in Appendix B, pages B46 and B47. Therefore, impacts are not expected.

**Wetland D**

Wetland D is a PEM wetland located within the western survey area (Des. No. 1800255) on the south side of SR 46 south of Wetland C. This wetland was considered to be poor quality primarily due to its small size, low species diversity, and disturbance from the roadway. Approximately 0.012 acre exists within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. However, Wetland D is located outside the construction limits for this project as shown on the project plan sheet

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in Appendix B, page B47. Therefore, impacts are not expected.

**Wetland E**

Wetland E is a PEM wetland located within the western survey area (Des. No. 1800255), approximately 100 feet east of Central Railroad Company of Indiana. This wetland was considered to be poor quality primarily due to its small size, low species diversity, and disturbance from the roadway. Approximately 0.041 acre exists within the survey area used for the *Waters of the U.S. Determination / Wetland Delineation Report*. Approximately 0.01 acre of permanent impacts will occur to Wetland E for the installation of new storm sewer pipes and regrading as shown on project plans in Appendix B, page B53. There will be no temporary impacts to Wetland E as a result of this project.

Mitigation is not anticipated to be required as wetland impacts are not anticipated to exceed 0.1 acre. Waterway permits will be required for impacts to Wetland E. Please refer to the *Permits* section of this CE documents for more details.

Wetlands A, B, C, D, and the portion of Wetland E that will not be impacted, will be labeled on the plans as "Do Not Disturb". This is included as a firm commitment in the *Environmental Commitments* section of this document.

**Early Coordination**

The USFWS responded on September 20, 2021, stating that wetland impacts may require permits from the USACE, IDEM WQC program, and the IDNR (Appendix C, pages C28 to C29). They went on to state that wetland impacts should be avoided, and any unavoidable impacts should be compensated for in accordance with the USACE mitigation guidelines.

The IDNR-DFW responded on October 13, 2021, stating that due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program. They went on to state that impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding. All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		Yes	NO
<b>Terrestrial Habitat</b>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 5 Acre(s)      Total tree clearing: 0.16 Acre(s)

*Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.*

Based on a desktop review, a site visit on June 09, 2021 by RQAW, the aerial maps of the project area (Appendix B, pages B3 to B14), there is roadside and lawn habitat within the project area. In addition, there is some forested riparian habitat present adjacent to the project area within the limits of Des No. 1800255; however, no impacts will occur to forested riparian habitat as work in the area surrounding the habitat will be confined to the pavements edge. Impacts to roadside and lawn habitat will occur as a result of this project. Approximately 5 acres of roadside and lawn habitat exist within the project area. Dominant vegetation consists of white leaf clover (*Trifolium repens*), annual blue grass (*Poa annua*), white mulberry (*Morus alba*), sugar maple (*Acer saccharum*), silver maple (*Acer saccharinum*) and black walnut (*Juglans nigra*). Impacts will occur from construction of new stormwater system, water main installation, permanent lighting installation, reconstruction of ADA curb ramps, and installation of new multi-use path. Up to 0.16 acre of tree trimming/removal will occur. Complete avoidance of tree removal is not possible as tree removal is necessary for the project to meet the purpose and need. However, tree removal has been minimized to the greatest extent possible. Mitigation is not anticipated for these impacts as no tree trimming/clearing will take place within a floodway and there is no permit or regulation that requires it. However, all trees removed within the Greensburg Eastside Residential Historic District will be replaced in the grass buffer. This is included as a firm commitment in the *Environmental Commitments* section of the CE. Approximately 10.7 acres of soil disturbance will occur with this project and an IDEM Construction Stormwater General Permit (CSGP) will be required. Please refer to the *Permits* section of this CE document for more information.

**Early Coordination**

The USFWS responded on September 20, 2021, providing standard recommendations to reduce or avoid impacts to terrestrial habitat (Appendix C, pages C28 to C29). Standard recommendations include implementing temporary erosion and sediment control measures, restricting tree clearing limits, and limiting vegetation clearing. All applicable recommendations can be found in the

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The IDNR-DFW responded on October 13, 2021, providing standard recommendations to minimize, avoid, or compensate for impacts to terrestrial habitat. Standard recommendations include revegetating all bare and disturbed areas, minimizing clearing of trees and brush, avoid removing trees suitable for bats, avoid removal of urban trees and replacing any trees removed, tree mitigation in non-wetland areas, employ appropriately designed erosion and sediment control measures, and seed and protect all disturbed slopes or streambanks that are not currently protected (Appendix C, pages C23 to C25). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS:      NE       NLAA       LAA

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Migratory Birds**

Known usage or presence of birds (i.e. nests)  
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E, pages E1 to E17), completed by RQAW on March 25, 2021, the IDNR Decatur County Endangered, Threatened, and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated October 13, 2021 (Appendix C, pages C23 to C25) the Natural Heritage Program's Database has been checked and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. However, Greensburg City Park (Decatur County Parks & Rec) is located within 1/2 mile of the western portion of the project area. An INDOT 0.5 mile review occurred on January 24, 2020 and did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C31 to C45). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were generated in the IPaC species list other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on October 14, 2021, and based on the responses provided, the project was found to "*Not Likely to Adversely Affect*" the Indiana bat and/or the NLEB (Appendix C, page C47 to C59). INDOT reviewed and verified the effect finding on October 18, 2021, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) include the general AMM as well as lighting and tree removal restrictions. AMM's are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

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**Geological and Mineral Resources**

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): \_\_\_\_\_

*Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B2), the RFI report (Appendix E, pages E1 to E17), there are no karst features identified within or adjacent to the project area. In their early coordination response on September 13, 2021, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, pages C13 to C15). The IGWS stated the project is located within a floodway, high liquefaction potential, high potential for bedrock resources, low potential for sand and gravel resources, and that there is active or abandoned mineral resource extraction sites within 0.5 mile of the project area. The response from the IGWS was communicated to the project designer on November 3, 2021. Impacts are not expected.

The RFI report identified four petroleum wells that are located adjacent to the project area for Des No. 1800255 (Appendix E, page E4). The RFI recommended coordination with the IDNR Division of Oil and Gas. Coordination with the IDNR Oil and Gas Division occurred on September 13, 2021 and they responded on October 19, 2021 that there is one well within the project area (Appendix C, page C20). They went on to state that the well is located near the project area, but the location is approximated based on an old map. No real data is known on the well and was likely drilled around 1900. An inspector from the IDNR tried to locate the well on March 07, 2016 and determined that it was presumed to be plugged and could not be found. If this well is encountered during this project, it will need to be plugged properly. A firm commitment to this effect has been added to the *Environmental Commitments* section of this CE document.

## SECTION C – OTHER RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
<b>Drinking Water Resources</b>			
Wellhead Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Water Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Urbanized Area Boundary	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Public Water System(s)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project located in the St. Joseph Sole Source Aquifer (SSA):		<input checked="" type="checkbox"/>	<input type="checkbox"/>
If Yes, is the FHWA/EPA SSA MOU Applicable?		<input type="checkbox"/>	<input type="checkbox"/>
If Yes, is a Groundwater Assessment Required?		<input type="checkbox"/>	<input type="checkbox"/>

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

**Sole Source Aquifer**

This project is located in Decatur County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

**Wellhead Protection Area and Source Water**

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on September 13, 2021 by RQAW. This project is not located

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within a Wellhead Protection Area or Source Water Area. In an early coordination area letter dated September 17, 2021, IDEM stated the project is not located in a Wellhead Protection Area but it is within 2,000 feet of the City of Greensburg's Wellhead protection Area. In addition, they went on to state that this project is located in Westport Water Company's Source Water Assessment Area. An early coordination letter was sent to the City of Greensburg on September 13, 2021 (Appendix C, pages C1 to C4) as part of the original coordination effort to the same point of contact that IDEM provided which is the Board of Water superintendent. An early coordination letter was sent to the Westport Water Company on September 27, 2021 per IDEM's Groundwater section response. However, no response was received from either entity; therefore, it was assumed they have no concerns with this project. These features will not likely be impacted as all work will be conducted in accordance with a IDEM Construction Stormwater General Permit (formally known as a Rule 5 permit).

**Water Wells**

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on September 13, 2021 by RQAW. The nearest well is located adjacent to the north of the project area. The features will not likely be affected as they are outside of the construction limits for the project. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

**Urban Area Boundary (UAB)**

Based on a desktop review of INDOT's MS4 website (<https://entapps.indot.in.gov/MS4/>) by RQAW on September 13, 2021, this project is located within a UAB. An early coordination letter was sent on September 13, 2021 to the City of Greensburg MS4 coordinator. The MS4 coordinator did not respond within the 30-day time frame.

**Public Water System**

Based on a desktop review, a site visit on June 09, 2021 by RQAW, the aerial map of the project area (Appendix B, pages B3 to B14), this project is located where there is a public water system. The public water system will be affected as new water mains will be installed as part of this project. The water main work included with this project was proposed by and is funded by the City of Greensburg. An early coordination letter was sent to the Greensburg Board of Water on September 13, 2021; however, no response was received. Utility coordination will be ongoing as this project advances.

**Floodplains**

- Project located within a regulated floodplain
- Longitudinal encroachment
- Transverse encroachment
- Homes located in floodplain within 1000' up/downstream from project

**Presence**

**Impacts**

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1     Level 2     Level 3     Level 4     Level 5

*Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.*

Based on a desktop review of The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) by RQAW on September 13, 2021, and the RFI report (Appendix E, pages E1 to E17), this project is located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F36). An early coordination letter was sent on September 13, 2021, to the Local Floodplain Administrator. The floodplain administrator did not respond within the 30-day time frame.

This project qualifies as a Category 3 per the current INDOT CE Manual, which states, "The modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this

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encroachment is not substantial.”

<b>Farmland</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Prime Farmland (per NRCS)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Total Points (from Section VII of CPA-106/AD-1006*)	<u>N/A</u>		
<i>*If 160 or greater, see CE Manual for guidance.</i>			

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on June 09, 2021 by RQAW, the aerial map of the project area (Appendix B, pages B3 to B14), there is farmland as defined by the Farmland Protection Policy Act adjacent to the project. This project will not convert any farmland, as all farmland is located within the western limits of Des No. 1800255 where all work will be confined to the pavements edge and no temporary or permanent right-of-way will be required. An early coordination letter was sent on September 13, 2021 to the Natural Resources Conservation Service (NRCS); in which they responded that Des No. 18000256 would not cause a conversion of prime farmland, but Des No. 1800255 would (Appendix C, page C21). Additional coordination with the NRCS occurred on October 1, 2021 explaining that this project would not require any right-of-way from farmland. The NRCS responded to the additional coordination on October 14, 2021 that Des No's. 1800255 and 1800256 would not cause a conversion of prime farmland (Appendix C, page C22). No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

### SECTION D – CULTURAL RESOURCES

Minor Projects PA  Category(ies) and Type(s)  INDOT Approval Date(s)  N/A

Full 106 Effect Finding  
 No Historic Properties Affected  No Adverse Effect  Adverse Effect

Eligible and/or Listed Resources Present  
 NRHP Building/Site/District(s)  Archaeology  NRHP Bridge(s)

Documentation Prepared (mark all that apply)	<input type="checkbox"/>	<u>ESD Approval Date(s)</u>	<u>SHPO Approval Date(s)</u>
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>	July 20, 2022	August 22, 2022
800.11 Documentation	<input checked="" type="checkbox"/>	July 20, 2022	August 22, 2022
Historic Properties Report or Short Report	<input checked="" type="checkbox"/>	February 17, 2021	March 22, 2021
Archaeological Records Check and Assessment	<input type="checkbox"/>		
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	February 17, 2021	March 22, 2021
Archaeological Phase Ic Survey Report	<input type="checkbox"/>		
Other:	<input type="checkbox"/>		

Memorandum of Agreement (MOA)  **MOA Signature Dates** (List all signatories)  
 FHWA: December 20, 2022  
 Indiana SHPO: December 12, 2022  
 INDOT: November 16, 2022  
 City of Greensburg: November 17, 2022

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

**Area of Potential Effect (APE):**

The APE is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE includes the existing and proposed right-of-way, immediately adjacent properties, and those areas where a visual differentiation may occur between the existing facilities and the project area. The above-ground APE is a discontinuous, irregular polygon that extends one property from the project area (approximately 200 feet from either side of the road centerline). Please refer to Appendix D, pages D27 to D28 for maps that show the above-ground APE boundaries. The archaeology APE for this project is defined by the approximately 21 acres of new, temporary, and existing right-of-way investigated for the presence of archaeological deposits.

**Coordination with Consulting Parties:**

Early coordination was initiated with potential consulting parties on February 12, 2020 by RQAW. The early coordination letter was either mailed or emailed and invited agencies or individuals to be consulting parties (Appendix D, pages D115 to D119). Early coordination was initiated with tribal contacts by the INDOT Cultural Resources Office (CRO) on February 12, 2020 (Appendix D, pages D120 and D121). The following is a list of organizations and individuals that were sent early coordination letters. Those who indicated they wished to be consulting parties are in bold. [Note: The Indiana State Historic Preservation Officer (SHPO) is an automatic consulting party. The FHWA is the federal agency undertaking the project with INDOT acting on behalf of the FHWA]. See the complete list of consulting parties in Appendix D, pages D110 and D113, and consulting parties' correspondence in Appendix D, pages D115 to D231.

Section 106 Consulting Party	Date of Response(s)
<b>Indiana Landmarks Eastern Regional Office</b>	<b>February 15 and 17, 2022</b>
Decatur County Alliance for Preservation	No Response
Main Street Greensburg	No Response
Decatur County Historian	No Response
Greensburg-Decatur County Public Library/ Decatur County History Blog	No Response
Historical Society of Decatur County	No Response
Decatur County Commissioner Members	No Response
Decatur County Surveyor	No Response
Decatur County Highway Supervisor	No Response
Greensburg City Engineer	No Response
<b><sup>2</sup>Greensburg Mayor</b>	<b>February 14, 2020</b>
Greensburg Street Commissioner	No Response
<b><sup>2</sup>Building Commissioner and former Greensburg Historic Preservation Commission</b>	<b>February 14, 2020</b>
Greensburg City Council Members	No Response
Property Owners of the Charles Zoller House	No Response
Historic Michigan Road	No Response
Eastside Residential Historic District Property Owners	No Response
<b><sup>1</sup>Eastern Shawnee Tribe of Oklahoma</b>	<b>January 11, 2022</b>
<b>Miami Tribe of Oklahoma</b>	<b>March 12, 2020</b>
<b><sup>1</sup>Peoria Tribe of Indians of Oklahoma</b>	<b>January 18, 2022</b>
Pokagon Band of Potawatomi Indians	No Response
Delaware Tribe of Indians	No Response
Shawnee Tribe	No Response

<sup>1</sup>The Indiana Landmarks Eastern Regional Office, the Eastern Shawnee Tribe of Oklahoma, and Peoria Tribe of Indians of Oklahoma agreed to be a consulting party as a result of a later coordination letter that was sent out on January 11, 2022 (Appendix D, page D217 to D220 and D223 to D224).

<sup>2</sup> In an email dated February 14, 2020, Joshua Marsh, the Mayor of The City of Greensburg, accepted the consulting party invitation. Additionally, Mayor Marsh requested the inclusion of Sarah Hamer, the Building Commissioner, and the Historical Preservation Committee Chair, in future coordination regarding this project (Appendix D, page D122).

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In a letter dated March 12, 2020, Diane Hunter, on behalf of the Miami Tribe of Oklahoma, accepted the consulting party invitation (Appendix D, page D123).

In a letter dated March 13, 2020, the Indiana SHPO staff indicated that if right-of-way is likely to be taken from a potentially historic property, they advised inviting the owner of that property as soon as possible. Therefore, the property owners of the Charles Zoller House were mailed the coordination letter on February 18, 2021. Additionally, the SHPO staff noted the SR 46 Pavement Replacement Project (Des. Nos. 1800255 & 1800256) overlaps with the then current SR 46 over Gas Creek Culvert Project (Des. No. 1400150) and requested that any limestone curbs within a listed or eligible historic district be evaluated in the Historic Property Report (HPR).

No other responses to the February 12, 2020 early coordination letter were received.

### Archaeology:

An archaeology report (Phase Ia Archaeological Reconnaissance) was completed by a qualified professional from Cultural Resource Analysts, Inc. (Harth, 12/18/2020). The reconnaissance identified three previously unrecorded sites; however, all three sites exhibited poor archaeological integrity and recommended that they are not eligible for inclusion in the National Register of Historic Places (NRHP). Therefore, no further work was recommended and archaeological clearance was recommended for this proposed project (Appendix D, pages D236 to D238). The INDOT-CRO approved the archaeology report on February 17, 2021. The archaeological report was sent to the Indiana SHPO on February 18, 2021. In a letter dated March 22, 2021 the Indiana SHPO concurred with the findings of the archaeological report (Appendix D, pages D133 to D135).

No other responses to the archaeology report were received.

### Historic Properties:

The APE was investigated for the existence of any historic properties and/or structures by a qualified professional from RQAW on February 11, 2020. Per the field visit and associated documentary research, the historian identified three properties as listed in, or eligible for, the NRHP.

- **Charles Zoller House (NR-0308/ IHSSI # 031-252-24091/ 345 E. Main Street):** The NRHP nomination process was initiated in the 1970s for the 1883 Charles Zoller House but was not completed. After the NRHP nomination process was initiated, the property was listed in the State Register in 1980, for its significance under Criterion A in the area of Exploration/Settlement and Criterion C in the area of Architecture. The two-and-one-half-story Neo-Jacobean style house features locally made red brick construction with horizontal bands and blocks of grey limestone. A glazed tile pattern is set flush in the outer wall of the entry porch on the first floor. The house features an asymmetrical plan with gables, dormers, balconies. The form incorporates an irregular rectangle with projecting sections and bays. Several of the original porches and a balcony are missing. The original slate roof has also been replaced with asphalt shingles; however, the ornamental wood crestings over the gables, dormers, and balconies were retained. The property occupies approximately 2.5 acres located on the west side of Gas Creek, and 1.5 blocks from the Greensburg Downtown Historic District. The Charles Zoller House retains two contributing outbuildings: a carriage house and a spring house. Both buildings sit south of the house and retain original features such as wood windows and doors. In addition, landscape features include sections of limestone walls separated by two sets of limestone steps that lead up to the main entrance. Wrought iron handrails are set into the steps. These features contribute to the overall aesthetic of the historic property.
- **Greensburg Downtown Historic District (NR- 1261/ IHSSI # 031-252-21001):** The Greensburg Downtown Historic District is roughly bounded by W. North Street, Railroad Street, the alley between N. East and N. Lincoln streets, and the alley between N. Broadway and N. Jackson streets in the City of Greensburg. The Greensburg Downtown Historic District was listed in the NRHP in September 1995 for significance under Criterion A in the areas of Commerce and Politics/Government and Criterion C in the area of Architecture. The district is centered on the courthouse square and extends approximately one block north, south, and east, and one-half block west. It is primarily composed of one to three-story commercial and government buildings constructed between about 1854 and 1945. The district is representative of Greensburg's commercial development since the 1850s through its Italianate, Queen Anne, and Classical Revival architectural styles. While there are some gaps in the architectural fabric due to a fire in the mid-1950s and demolitions since the NRHP listing, the characteristics of the district that make it eligible remain intact.
- **Greensburg Eastside Residential Historic District (IHSSI # 031-252-23001-068):** The Greensburg Eastside Residential Historic District is located east of downtown Greensburg. It incorporates the houses along Main Street and Washington Street from Lincoln Street east to Stewart Street and continues east along Main Street and the south side of Washington Street to Warren Street. The district contains some of the earliest homes in Greensburg and is generally composed of residences constructed between c. 1860 and the 1940s. The district portrays stylistic change over time and exhibits both

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high-style architecture of Greensburg's wealthy citizens and middle-class vernacular styles that fill in the now-subdivided lots, accounting for Greensburg's continued growth. The district once featured a small stone arch structure that was previously surveyed through the analysis and documentation process for the SR 46 Small Structure Replacement Project over Gas Creek (Des. No. 1400150). The structure was extant in February 2020 during the field investigation but was removed during the construction of Des. No. 1400150. A stone bench and informational sign were constructed to mitigate the impacts of the structure's removal. In addition, there are noncontinuous sections of limestone curbs within the project area that likely date to the time when the road was originally paved during the late nineteenth century or early twentieth century. The limestone curbs are a character defining feature of the district and contribute to its overall aesthetic. Since the time of its initial survey through the IHSSI, the district's historic integrity has remained high.

Qualified professionals from RQAW compiled information from the site visit and research regarding historic resources into a Historic Property Report (HPR, Boot/Moscato, February 17, 2021) (Appendix D, pages D234 to D235). The HPR was sent to consulting parties, including the Indiana SHPO and property owners of the Zoller House, on February 18, 2021 (Appendix D, pages D126 to D132). A weblink to the HPR was also provided at this time. In a letter dated March 22, 2021, the Indiana SHPO concurred with the recommendations of the HPR (Appendix D, pages D133 to D135) and expressed concern with the protection and avoidance of impacts to the historic landscape features within the Greensburg Eastside Residential Historic District, such as mature trees, stone retaining walls, limestone curbs and brick sidewalks.

No other responses to the HPR were received.

### **Documentation Findings:**

The effects letter was approved by INDOT-CRO on July 6, 2021. On July 22, 2021, a hard copy of the Effects Letter was mailed to the Indiana SHPO, while other consulting parties were informed via email that the letter could be viewed electronically by accessing IN SCOPE (INDOT's online portal for public viewing Section 106 documents). The letter identified potential effects to the historic resources and requested comments from consulting parties. The effects letter also invited the consulting parties to attend a consulting party meeting on August 11, 2021. Please refer to Appendix D, pages D136 to D145 for a copy of the effects letter and Appendix D, pages D172 to D174 for email correspondence sent to consulting parties. Prior to the consulting party meeting, an email invitation was sent to consulting parties on July 27, 2021, notifying them the meeting will be held virtually through an online platform (Appendix D, pages D175 and D176). The invitation included the link to participate in the meeting online with video and/or phone and the meeting agenda.

The consulting party meeting was held virtually on August 11, 2021 via an online meeting platform. The meeting discussed the purpose and need, existing conditions, preliminary alternative, previous Section 106 coordination, identified historic resources within the APE, and potential effects to historic properties. The consulting party meeting minutes were provide to the consulting parties for review and comment on August 27, 2021 (Appendix D, pages D182 to D195).

In a letter dated August 23, 2021, the Indiana SHPO concurred with the Effects Letter stating, that they agree that the Greensburg Downtown Historic District would not be adversely affected by the project as currently designed. They went on to state that they agree that the Greensburg Eastside Historic District would be adversely affected due to the loss of trees, removal of limestone curbs and other changes in the landscape, such as the introduction of a multi-use path. In regard to the Charles Zoller House, they stated that they believe that it may be possible to avoid or greatly minimize potential adverse effects to the property through preservation of the stone retaining wall and steps, replacement of the existing concrete sidewalk in-kind and possible resetting of limestone curb. Within the Greensburg Eastside Historic District, the SHPO was concerned about the removal of the existing concrete sidewalk for the placement of a ten-foot-wide multi-use path on the north side of SR 46. They requested that consideration be given to reducing the width of the path and using concrete instead of asphalt to more closely resemble the 'higher grade' material and finish of the historic sidewalk. They also stated that it would be appropriate to replicate the stamped street names in the path if it is a typical feature in the district.

The SHPO concluded their response to the Effects Letter by stating they look forward to receiving additional information on placement of street lighting and feasibility of replanting trees and resetting limestone curbs. It was suggested, during the consulting party meeting that the Wilderwood House and the Charles Zoller House may be prioritized for replacement of limestone curbing given their outstanding historic and architectural significance. Mitigation ideas also include survey and/or National Register nomination for the Greensburg Eastside Residential Historic District. The Indiana SHPO stated that they are supportive of these mitigation proposals and are interested to learn the views of residents and affected property owners on the proposed project and the desirability of potential mitigation measures (Appendix D, pages D178 to D180).

No other responses to the Effects Letter were received.

In response to the Indiana SHPO staff letter, dated August 23, 2021, expressing interest in the views of the affected residents and

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property owners on the proposed project and the desirability of potential mitigation measures, a coordination letter and residents' survey were prepared. On January 11, 2022, a hard copy of the coordination letter and survey were mailed to the Indiana SHPO and property owners and residents of the Greensburg Eastside Residential Historic District along SR 46 (Main Street), while other consulting parties were informed via email that the letter could be viewed electronically by accessing IN SCOPE (Appendix D, pages D196 to D207).

In a letter dated January 11, 2022, Paul Barton on behalf of the Eastern Shawnee Tribe of Oklahoma responded stating the project proposes No Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe (Appendix D, page D219).

In a letter dated January 18, 2022, Charla K. EchoHawk on behalf of the Peoria Tribe of Indians of Oklahoma responded stating the Peoria Tribe has no objection at this time to the proposed project (Appendix D, page D220).

In a letter dated February 10, 2022, the Indiana SHPO staff responded to the January 11, 2022 letter by stating, they appreciate the efforts to keep them involved in the consultation for this project and they look forward to learning the views of other consulting parties and residents on the minimization and mitigation proposals. (Appendix D, pages D221 to D222).

In email correspondence between February 15 and 17, 2022, Brittany Miller of the Indiana Landmarks Eastern Regional Office offered no comments to the Section 106 materials and asked to be included on future correspondence (Appendix D, pages D223 to D225).

Survey responses were received from five property owners/residents of the Greensburg Eastside Residential Historic District between January 16 and February 23, 2022. Overall, there was a general consensus that the multi-use path be constructed with concrete rather than asphalt. Additionally, two respondents indicated that reducing the path width to 8-feet (from 10-feet wide) would mitigate/reduce impacts to the district. One respondent was indifferent, while two did not feel that reducing the path width to 8-feet (from 10-feet wide) would mitigate/reduce impacts to the district. Regarding street trees, respondents were unanimously in favor of replanting street trees. Lastly, support for an updated survey of the district was expressed and respondents were indifferent to using informational signage as mitigation for project impacts to the district. Please refer to Appendix D, pages D226 to D232 for all survey response correspondence.

No other responses to the coordination letter or residents' survey were received.

The 800.11 documentation for the "Adverse Effect" Section 106 finding was signed by the FHWA, on July 20, 2022 (Appendix D, pages D1 to D281). The finding stated that the project would have an "Adverse Effect" on the Greensburg Eastside Residential Historic District and would have "No Adverse Effect" on the Greensburg Downtown Historic District and the Charles Zoller House.

A draft MOA, dated July 20, 2022, was prepared to resolve the "Adverse Effect" on the Greensburg Eastside Residential Historic District.

The "Adverse Effect" finding, draft MOA, and e106 form were sent to the Advisory Council on Historic Preservation (ACHP), on July 20, 2022 (Appendix D, page D291). Furthermore, the "Adverse Effect" finding and a draft MOA were sent to consulting parties, including the Indiana SHPO, on July 20, 2022. In a letter dated August 22, 2022, the Indiana SHPO concurred with the "Adverse Effect" Section 106 finding (Appendix D, pages D294 to D296), and in a letter dated August 26, 2022, the ACHP responded that because they "did not respond within 15 days with a decision regarding our non-participation, that the ACHP assumes that the FHWA has continued the consultation to resolve adverse effects" (Appendix D, page D297). The Miami Tribe of Oklahoma responded to the "Adverse Effect" finding in a letter dated December 13, 2022, offered no objection to the project (Appendix D, page D298).

In the letter dated August 22, 2022, the Indiana SHPO also requested a change to the draft MOA, stating, to include a reference to 312 IAC 22 regarding any necessary archaeological investigations under section *III. Post-Review Discovery* of the draft MOA. The Indiana SHPO, went on to state that other than this revision, they find the conditions and stipulations of the draft MOA acceptable (Appendix D, pages D294 to D296).

The revised MOA was approved by INDOT-CRO and signed by INDOT on November 16, 2022. The revised MOA was sent to the signatories (including Indiana SHPO and Mayor of Greensburg) on November 17, 2022 (Appendix D, page D299). The SHPO concurred and signed the MOA on December 12, 2022, and provided a Letter of Clearance (Appendix D, pages D300 to D301). The Mayor of Greensburg signed the MOA on November 17, 2022 (Appendix D, page D310). The FHWA signed the MOA on December 20, 2022 (Appendix D, page D307).

The fully executed MOA (Appendix D, pages D302 to D310) was distributed to consulting parties on January 3, 2023 (Appendix D,

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pages D311 to D318), and was filed with the ACHP via an email dated January 3, 2023 (Appendix D, page D319). The Eastern Shawnee Tribe responded to the fully executed MOA on January 24, 2023, without any concerns or objections to the project (Appendix D, page D320). No other responses were received.

**Public Involvement:**

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "Adverse Affect" was published online at: Greensburg Daily News Public Notices and in the Greensburg Daily News on July 26, 2022, offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed at least 30 days later, on August 30, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages D292 to D293. No comments were received.

This completes the Section 106 process and responsibilities of the FHWA under Section 106 have been fulfilled.

### SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		<u>Yes</u>	<u>No</u>
<b>Parks and Other Recreational Land</b>			
Publicly owned park	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b><u>Evaluations Prepared</u></b>			
Programmatic Section 4(f)	<input type="checkbox"/>		
"De minimis" Impact	<input type="checkbox"/>		
Individual Section 4(f)	<input type="checkbox"/>		
Any exception included in 23 CFR 774.13	<input checked="" type="checkbox"/>		

*Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.*

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, aerial map of the project area (Appendix B, pages B3 to B14), and the RFI report (Appendix E, pages E1 to E17) there are 22 potential 4(f) resources located within the 0.5 mile search radius. According to additional research, and by the site visit on June 14, 2021 by RQAW, there are four Section 4(f) resources and two potential 4(f) resources within or adjacent to the project area.

- **Charles Zoller House (NR-0308/ IHSSI # 031-252-24091):** Qualifies as a Section 4(f) resources as it is eligible for listing in the NRHP.
- **Greensburg Downtown Historic District (NR- 1261/ IHSSI # 031-252-21001):** This historic district qualifies as a Section 4(f) resource is it is listed in the NRHP.
- **Greensburg Eastside Residential Historic District (IHSSI # 031-252-23001-068):** This historic district qualifies as a

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Section 4(f) resources as it is eligible for listing in the NRHP.

- **Greensburg City Park/Decatur County Park:** Qualifies as a Section 4(f) resource as it is a publicly owned park.
- **Greensburg Country Club:** This facility is managed and operated by a private entity, but it is owned by a public agency, open to public, and considered to be a significant public recreational area; therefore, it would potentially qualify as a Section 4(f) facility.
- **Greensburg Bicycle/Pedestrian Path:** This path is a recreational feature that would potentially qualify as a Section 4(f) resource but is currently listed as a planned trail segment.

### **Charles Zoller House**

According to the 800.11 documentation (Appendix D, page D3), a “No Adverse Effect” finding was made for this historic resource. The Charles Zoller House is located south of SR 46 (Main Street) and west of Gas Creek in the City of Greensburg, Indiana; it occupies approximately 2.5 acres. Temporary right-of-way will be needed from this resource for construction activities associated with sidewalk reconstruction, drive reconstruction, and grade work. As such, this project will temporarily occupy land from this property, but does not constitute a Section 4(f) use as long as all the below criteria are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

As part of the 800.11 documentation the FHWA requests the Indiana State Historic Preservation Officer provide written concurrence that they are in agreement that the above criteria have been met and that the impacts to the Charles Zoller House constitute a temporary occupancy. The work at this location will be less than the time needed to construct the project. Additionally, the scope of work at this location will be minor and consists of sidewalk reconstruction, drive reconstruction, and grade work. There will be no permanent adverse impacts to this resource and the project will not interfere with activities, features, or attributes of the property. Lastly, the land will be restored following construction activities. Please refer to Appendix D, page D13 for a description of the project effects on the Charles Zoller House and how the above conditions 1-4 are met. On August 22, 2022 the Indiana SHPO (Official with Jurisdiction or OWJ) concurred that this project meets the above criteria; which satisfies condition 5. therefore, no Section 4(f) use will occur (Appendix D, pages D284 to D286).

### **Greensburg Downtown Historic District:**

The Greensburg Downtown Historic District is centered on the Decatur County Courthouse square and extends approximately one block north, south, and east, and one-half block west in the City of Greensburg, Indiana. This project will not convert any property from this district; therefore, no Section 4(f) evaluation is needed. No use will occur.

### **Greensburg Eastside Historic District:**

According to the 800.11 documentation (Appendix D, page D3), an “Adverse Effect” finding was made for this historic resource The Greensburg Eastside Historic District is located just east of downtown Greensburg in the Town of Greensburg, Decatur County. It incorporates the houses along Main Street and Washington Street from Lincoln Street east to Stewart Street and continues east along Main Street and the south side of Washington Street to Warren Street. Within this historic district, both reacquisition and temporary right-of-way will be required. This work includes, but not limited to, replacement of sidewalk with a multi-use path, replacement of curb ramps, storm sewer work, full depth pavement replacement, installing/replacing curb and gutter, and installation of new street lighting. Please refer to Appendix D, pages D13 to D15 for all work that will occur within this historic district. As such, this project will temporarily occupy land from this property, but does not constitute a Section 4(f) use as long as all the below criteria are satisfied:

1. Duration must be temporary, i.e., less than the time needed for construction of the project, and there should be no change in ownership of the land.
2. Scope of the work must be minor, i.e., both the nature and the magnitude of the changes to the Section 4(f) property are minimal;
3. There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
4. The land being used must be fully restored, i.e., the property must be returned to a condition which is at least as good

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- as that which existed prior to the project; and
5. There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

As part of the 800.11 documentation the FHWA requests the Indiana State Historic Preservation Officer provide written concurrence that they are in agreement that the above criteria have been met and that the impacts to the Greensburg Eastside Historic District constitute a temporary occupancy. The work at this location will be less than the time needed to construct the project. Additionally, the scope of work at this location will consist of, but not limited to, replacement of sidewalk with a multi-use path, replacement of curb ramps, storm sewer work, full depth pavement replacement, installing/replacing curb and gutter, and installation of new street lighting. There will be no permanent adverse impacts to this resource and the project will not interfere with activities, features, or attributes of the property. Lastly, the land will be restored following construction activities. Please refer to Appendix D, pages D13 to D15, for a description of the project effects on the Greensburg Eastside Historic District and how the above conditions 1-4 are met. On August 22, 2022 the Indiana SHPO (OWJ) concurred that this project meets the above criteria; which satisfies condition 5. Therefore, no Section 4(f) use will occur (Appendix D, pages D284 to D286).

### **Greensburg City Park/Decatur County Park:**

This park is located just south of the project area (Des No. 1800255) at S. County Road 200 W., approximately 900 feet east of the SR 3 and SR 46 intersection. This property qualifies as a Section 4(f) resource as it is a publicly owned park. However, the work to occur at this location will be confined to the pavements edge and no permanent or temporary ROW is needed from this property. Therefore, no Section 4(f) use will occur. Access to this property will be maintained at all times during construction. Please refer to Appendix B, page B3 for the location of this park.

### **Greensburg Country Club:**

The Greensburg Country Club golf facility is located within the boundaries of the Greensburg City Park/Decatur County Park and is leased to club members by the City of Greensburg. This golf facility was once a semi-private club but is currently open to the public and hosts a variety of leagues and tournaments, including but not limited to junior high and senior high school teams from the immediate community and the entire State of Indiana. Even though this facility is managed and operated by a private entity, it is owned by a public agency, open to public, and considered to be a significant public recreational area; therefore, it likely qualifies as a Section 4(f) facility. However, the work to occur at this location will be confined to the pavements edge and no permanent or temporary ROW is needed from this property. Therefore, no Section 4(f) use will occur. Access to this property will be maintained at all times during construction. Please refer to Appendix B, page B3 for the location of this country club.

### **Decatur County Youth Sports Complex:**

This sports complex is located south of the project area (Des No. 1800255) at S. County Road 200 W., approximately 800 feet east of the SR 3 and SR 46 intersection (just west of the Greensburg City Park/Decatur County Park). This park is owned and managed by a public agency and would likely qualify as a Section 4(f) resource as the facility is used primarily for public recreation. However, the work to occur at this location will be confined to the pavements edge and no permanent or temporary ROW is needed from this property. Therefore, no Section 4(f) use will occur. Access to this property will be maintained at all times during construction. Please refer to Appendix B, page B3 for the location of this sports complex.

### **Greensburg Bicycle/Pedestrian Path:**

The Greensburg Bicycle/Pedestrian Path is a planned trail segment according to GIS, (rail-trail along former Conrail railroad) that is located just south of the project area (Des No. 1800255) at the northern boundaries of the Greensburg City Park/Decatur County Park. According to the Greensburg Bicycle and Pedestrian System Plan, the Greensburg Bicycle/Pedestrian Path is not listed or shown (Appendix I, page I35). This trail segment is not currently funded, nor is there any plans to construct this trail in the foreseeable future. Additionally, this project would not impede or restrict this trail from being constructed in the future. Therefore, no Section 4(f) use will occur.

### **Early Coordination**

Coordination occurred with the Decatur County Parks and Recreation Department and the Greensburg Country Club occurred on September 13, 2021 (Appendix C, page C1 to C4). No response was received from either entity.

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**Section 6(f) Involvement**

**Presence**

**Use**

**Section 6(f) Property**

Yes

No

*Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.*

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed that one property is present in Decatur County (Appendix I, page I1). The Decatur County Park is located adjacent to the project area; however, all work will be confined to the pavements edge at the location of the park and no permanent or temporary right-of-way will be required from the park. Therefore, there will be no impacts to 6(f) resources. Access to this property will be maintained at all times during construction. Please refer to Appendix B, page B3 for the location of the Decatur County Park.

### SECTION F – Air Quality

**STIP/TIP and Conformity Status of the Project**

- Is the project in the most current STIP/TIP?
- Is the project located in an MPO Area?
- Is the project in an air quality non-attainment or maintenance area?
- If Yes, then:
  - Is the project in the most current MPO TIP?
  - Is the project exempt from conformity?
- If No, then:
  - Is the project in the Transportation Plan (TP)?
  - Is a hot spot analysis required (CO/PM)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP: FY 2022-2026 STIP (Initial)  
 Name of MPO (if applicable): N/A  
 Location in TIP (if applicable): N/A

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

**STIP**

The Fiscal Year (FY) 2022-2026 Statewide Transportation Improvement Program (STIP) is listed based on the lead DES number in the contract. The lead Des number for this contract is 1800256. The FY 2022-2026 STIP includes Des No. 1800255 by reference with contract number R-41463 (Appendix H, page H1).

**Attainment Status**

This project is located in Decatur County, which is currently in attainment for all criteria pollutants according to the Environmental Protection Agency (EPA) website (<https://www.epa.gov/green-book>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

**Mobile Source Air Toxics (MSAT) analysis**

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act

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conformity rule under 40 CFR 93.126, and as such, a MSAT analysis is not required.

## SECTION G - NOISE

**Noise**

**Yes      No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?       

Date Noise Analysis was approved/technically sufficient by INDOT ESD: \_\_\_\_\_

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

## SECTION H – COMMUNITY IMPACTS

**Regional, Community & Neighborhood Factors**

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
- If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

**Regional, Community and Neighborhood Factors**

The project will comply with the local/regional development patterns for the area. The project is not anticipated to result in substantial impacts to community cohesion because it will not change access to properties within the area or divide existing communities. Replacing sidewalks and curb ramps to meet current ADA criteria, as well as providing a multi-use path will increase cohesion. The proposed project is not expected to negatively impact the surrounding community or cause negative economic impacts to the surrounding area. Therefore, the project will have minimal or no negative impacts to the community or local economy.

The City of Greensburg has an approved ADA Transition plan dated September 30, 2011 available at: [Greensburg ADA Transition Plan](#). This project will comply with this plan by ensuring that all sidewalks and curb ramps within the limits of the project will meet current ADA requirements.

**Public Facilities and Services**

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.*

Based on a desktop review, the aerial map of the project area (Appendix B, pages B3 to B14), and the RFI report (Appendix E, pages E1 to E17) there are 41 public facilities (9 religious facilities, 1 public airport, 1 hospital, 4 schools, 13 recreational facilities, 8 pipeline segments, 4 trails, and 1 managed land) within the 0.5 mile search radius. There are 11 public facilities (1 airport, 8 pipeline segments, 3 recreational facilities, and 1 managed land) within or adjacent to the project area confirmed by the site visit on June 09, 2021 by RQAW, and the project plans (Appendix B, pages B30 to B149).

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### **Public Airport**

The Greensburg Municipal Airport is located approximately 0.14 mile southwest of the western project terminus for Des No. 1800256.

### **Recreational Facilities**

The Oddfellows/Rebekah Park is located adjacent to the north of the eastern project terminus for Des No. 1800256. Two additional recreational facilities, The Greensburg City Park/Decatur County Park (also a managed land) and the Greensburg Country Club) are located adjacent to the south of the western project terminus for Des No. 1800255. There will be no permanent or temporary right-of-way taken from either of the above 3 recreation facilities and access to the properties will be maintained at all times during construction. Therefore, impacts are not expected.

Utility coordination will be ongoing as the project advances. According to the project designer, utility coordination to date has shown:

- City of Greensburg Water and Sewer: Will be impacted as described in the preferred alternative section of this CE document.
- Decatur County REMC: Not anticipated to require relocation.
- Duke Fiber: Will likely need to be relocated to grass buffer area from N. Lathorp Street to the end of the project area for Des No. 1800256.
- Duke Electric: It is anticipated that some utility poles are in conflict with the project. Utility pole relocations are anticipated within the right-of-way.
- Sprint and CenturyLink: May or may not require relocation. If relocations are necessary, the facilities are proposed to be relocated to the grass buffer area between the roadway and sidewalk.

If any utility relocations result in any additional environmental impacts that are not assessed in this environmental document, an Additional Information (AI) document will need to be prepared. A firm commitment to this effect has been added to the *Environmental Commitments* section of this CE document.

### **Early Coordination**

Early coordination letters were sent to both the Greensburg Board of Aviation and INDOT, Office of Aviation on September 13, 2021 (Appendix C, pages C1 to C4). The Greensburg Board of Aviation did not respond to early coordination. However, INDOT, Office of Aviation responded on September 15, 2021 stating that if any object, obstruction, or equipment will exceed 17 ft. in height, further coordination will be required with our office and the FAA. This is due to the close proximity of Greensburg Airport and the need for any obstructions within 5 miles to meet a 100:1 glideslope to the nearest runway according to 14 CFR Part 77 standards. However, additional coordination occurred October 1, 2021 and October 4, 2021 discussing the height restriction of 17 ft. for the entire project area. INDOT, Office of Aviation responded on October 4, 2021 stating that for Des No. 1800256 at the western most limit, a height of 75 ft can be achieved without having to file anything for tall structures. For Des 1800255, the 17 ft. limit still stands but it seems like that will be manageable for the project at hand. In their October 5, 2021 response they stated that for Des No. 1800255 the height limit of 17 ft. applies until you reach the Delta Faucet Building where 28 ft. height would be allowed, and where the Wallpe's Keg Restaurant is located a 38 ft. height would be allowed, at the Railroad tracks 42 ft. height would be allowed. They went on to state they could provide additional measurements if needed. Additional measurements were not requested. It is anticipated that this project will meet the height restrictions stated by INDOT, Office of Aviation. However, a firm commitment has been added to the *Environmental Commitments* section of this CE document stating that, if determined necessary, the designer shall coordinate further with INDOT, Office of Aviation and the FAA and obtain any required permits before Ready for Contracts (RFC).

In addition, early coordination letters were sent to the Decatur County Parks and Recreation Department (managing entity for the Oddfellows/Rebekah Park and The Greensburg City Park/Decatur County Park) as well as the Greensburg Country Club on September 13, 2021. No responses were received as part of early coordination efforts.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

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**Environmental Justice (EJ)** (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 0.82 acre of additional permanent right-of-way and no relocations. Therefore, an EJ analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Washington Township. The community that overlaps the project area is called the affected community (AC). In this project, there are two AC's and they are Census Tract 9692 and 9694. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the American Community Survey (ACS) 5-year estimates (2015-2019) was obtained from the U.S. Census Bureau's webpage at: <https://data.census.gov/cedsci/> on October 20, 2021 by RQAW. The data collected for minority and low-income populations within the AC's are summarized in the below table.

Table: Minority and Low-Income Data (ACS 5-Year Estimates 2015-2019)			
	COC – Washington Township, Decatur County, Indiana	AC-1 – Census Tract 9692, Decatur County, Indiana	AC-2 – Census Tract 9694, Decatur County, Indiana
Percent Minority	6.8%	8.2%	2%
125% of COC	8.6 %	AC < 125% COC	AC < 125% COC
EJ Population of Concern		No	No
Percent Low-Income	11.1%	12.4%	12.9%
125% of COC	13.9%	AC < 125% COC	AC < 125% COC
EJ Population of Concern		No	No

AC-1, Census Tract 9692 has a present minority of 8.2% which is below 50% and below the 125% COC threshold. AC-2, Census Tract 9694 has a present minority of 2% which is below 50% and below the 125% COC threshold. Therefore, both AC's do not contain minority populations of EJ concern.

AC-1 Census Tract 9692 has a present low income of 12.4% which is below 50% and below the 125% COC threshold. AC-2, Census Tract 9694 has a present low income of 12.9% which is below 50% and below the 125% COC. Therefore, both AC's do not contain low income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix I, pages I2 to I8. As no EJ populations of concern was identified, no further environmental justice analysis is warranted.

**Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations:      Residences:   0        Businesses:   0        Farms:   0        Other:   0  

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

**Relocation of People, Businesses, or Farms**

No relocations of people, businesses, or farms will take place because of this project.

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SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Documentation

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)

Table with 1 column and 4 rows, containing 'X' in the first row.

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

Date RFI concurrence by INDOT SAM (if applicable): March 26, 2021 and September 10, 2021

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Hazardous Materials and Regulated Substances

Based on a review of GIS and available public records, the RFI was completed by RQAW on March 25, 2021 and INDOT Site Assessment and Management (SAM) gave their concurrence on March 26, 2021 (Appendix E, pages E1 to E17). In addition, an addendum to the RFI was completed by RQAW on September 9, 2021 and INDOT SAM gave their concurrence on September 10, 2021 (Appendix E, pages E18 to E22). The addendum to the RFI was needed to document the addition of the SR 46 and US 421 west junction intersection reconstruction under Des No. 1800255 and only covered hazardous material concerns.

There are 4 Resource Conservation and Recovery Act (RCRA) Generator/ Treatment, Storage, and Disposal (TSD) sites, 4 State Cleanup sites, 21 Underground Storage Tanks (UST) sites, 1 Voluntary Remediation Program (VRP) site, 16 Leaking Underground Storage Tank (LUST) sites, 1 Confined Feeding Operations (CFO) site, 3 Brownfields sites, 10 Institutional Control Sites, and 4 National Pollutant Discharge Elimination System (NPDES) facilities located within 0.5 mile of the project area.

There are 6 UST sites, 2 LUST sites, 2 Institutional Control sites, 1 VRP site, 1 Brownfields site, and one NPDES facility that could affect the project.

UST Sites

Green Barn USA, SR 46 and Sunset Drive (AI ID 4640), is located adjacent to the north of the central portion of the project area for Des No. 1800255. According to the October 17, 1991 Notification of Underground Storage Tanks, one (1) 3,000-gallon UST of unknown age was taken out of service in October 1991. No other information regarding the current status of the tank was found in the IDEM VFC. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

Kiel Brothers Oil Bulk Plant, 1010 West Main Street (AI ID 3415), is mapped adjacent to the south of the central portion of the project area for Des No. 1800255. No information was found in the IDEM VFC for this UST site. The current status of the UST is unknown; therefore, in addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

Muncie Bread Company (Caldron Brothers Vending Incorporated) 117 South Anderson Street (AI ID 2828), is located approximately 0.02 miles south of the eastern portion of the project area for Des No. 1800255. According to the September 4, 1990 Notification of Underground Storage Tanks, one (1) 550-gallon UST was taken out of service in August 1990. The tank was removed from the ground; however, it does not appear as though confirmatory soil or groundwater samples were taken during UST removal. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

Scripture (Garry Hadler) CPD Incorporated (also listed as Water Tower Center), 325 West Main Street (AI ID 1768), is located adjacent to the southeast of the eastern project area terminus for Des No. 1800255. The UST was emptied and has not been in use since 1983. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

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Paul S Walston - 302 West Main Street (AI ID 3590), is located approximately 0.02 mile north east to the eastern project area terminus for Des No. 1800255. The UST site was a filling station dating back to 1935. The USTs have not been in use since 1971 and were abandoned in place (Filled with inert material). In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

**UST/LUST/Intuition Control Site:**

Ketchum's Shell, 106 South Michigan Avenue (AI ID 2294), is located adjacent to the southwest of the project area for Des No. 1800256. This is the site of an operating gas station. IDEM issued an NFA Determination Pursuant to RISC for the site on December 17, 2012. Low levels of soil and groundwater contamination remain on the site. An ERC was recorded on the property by Decatur County on September 4, 2012. The ERC specifically prohibits the use or extraction of groundwater at the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. Coordination will be conducted with the IDEM Project Manager, Kay Ifekoya ([kifekoya@idem.in.gov](mailto:kifekoya@idem.in.gov)) identified in the VFC documentation before further site activities occur.

**VRP:**

Advanced Bearing Materials LLC (KS Bearings), 1515 West Main Street (AI ID 15726), is located adjacent to the south of the western portion of the project area for Des No. 1800255. Remediation and monitoring activities are ongoing for this site. Documentation indicated that a large CVOC plume is present under portions of the site. A Phase II Environmental Site Assessment (ESA) is recommended. Prior to any investigation activities, a scope of work plan will be prepared and submitted to INDOT SAM for review and approval. However, additional coordination occurred with INDOT SAM on April 19, 2022 and they responded that based on the updated scope of work in the vicinity of Advance Bearing Materials, that they concur that a Phase II ESA is not warranted (Appendix E, pages E23 to E24).

**LUST/Institutional Controls:**

Former Kocolene Service Station, 601 West Main Street (AI ID 2326), is located adjacent to the south of the eastern portion of the project area for Des No. 1800255. This was formally the site of a gas station. IDEM issued an NFA Determination Pursuant to RISC for the site on June 29, 2017. Low levels of soil and groundwater contamination remain on site. An ERC was recorded on the property by Decatur County on September 29, 2016. The ERC specifically prohibits the use or extraction of groundwater, and limits excavation activities. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Project Manager, Jeff Turley at [jturley@idem.in.gov](mailto:jturley@idem.in.gov), identified in the VFC documentation before further site activities occur.

**Brownfields:**

Former Senior Center, 225 East Main Street (AI ID 6937), is located adjacent to the southwest of the western project area terminus for Des No. 1800256. According to the Site Status Letter issued by IDEM on January 23, 2008, sample results prior to remediation indicated soil and groundwater were impacted by TPH-GRO/ERO, VOCs, SVOCs, arsenic and lead. Current levels of contamination at the site meet current cleanup criteria; however, contamination is present off-site and potentially within the INDOT ROW. If excavation occurs in this area, it is likely that contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary.

**NPDES Facilities:**

Delta Faucet Company, 1425 West Main Street (AI ID 15043), is located adjacent to the south of the central portion of the project area for Des No. 1800255. Coordination with IDEM will occur.

Irvington Materials Incorporated, 1100 West Main Street (AI ID 15586), is located adjacent to the north of the central portion of the project area for Des No. 1800255. Coordination with IDEM will occur.

All of the above described sites that require additional coordination with IDEM and/or analysis for lead, have been added as firm commitments in the *Environmental Commitments* section of this CE.

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**Part IV – Permits and Commitments**

**PERMITS CHECKLIST**

Permits (mark all that apply)

Likely Required

**Army Corps of Engineers (404/Section10 Permit)**

Nationwide Permit (NWP)	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Other	<input type="checkbox"/>

**IN Department of Environmental Management (401/Rule 5)**

Nationwide Permit (NWP)	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>
Individual Permit (IP)	<input type="checkbox"/>
Isolated Wetlands	<input type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>

**IN Department of Natural Resources**

Construction in a Floodway	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>
Other	<input type="checkbox"/>

**Mitigation Required**

**US Coast Guard Section 9 Bridge Permit**

**Others (Please discuss in the discussion below)**

<input type="checkbox"/>

*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

As this project is anticipated to impact approximately 0.01 acre of Wetland E, a 404 USACE and 401 IDEM Nationwide Permit (NWP) is anticipated.

An IDEM Construction Stormwater General Permit (CSGP), formally known as a Rule 5 permit, will be required as soil disturbance associated with this project will be approximately 10.7 acres.

In their early coordination response dated October 13, 2021 the IDNR stated that This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of Muddy Fork Sand Creek or UNT 1 to Sand Creek, unless it qualifies for a bridge exemption or qualifies under the INDOT and IDNR Memorandum of Understanding (MOU) for Maintenance Activity Exemption, dated March 1997. This project does not qualify under the bridge exemption criteria as this project is within the city limits of Greensburg, plus no bridge work will occur. However, the HMA overlay work within the limits of the Muddy Fork Sand Creek floodway and full depth pavement replacement within the limits of the UNT 1 to Sand Creek (locally known as Gas Creek) will meet the conditions of the Maintenance MOU. In addition, the storm sewer work occurring within the limits of the UNT 1 to Sand Creek floodway meets the conditions of the IDNR Outfall General License; therefore, no Construction in a Floodway (CIF) permit is anticipated.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

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### ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

#### Firm:

1. If any utility relocations result in any additional environmental impacts that are not assessed in this environmental document, an Additional Information (AI) document will need to be prepared. (INDOT ESD)
2. If necessary, the designer shall coordinate further with INDOT, Office of Aviation in regards to height restrictions and the FAA and obtain any required permits before Ready for Contracts (RFC). (INDOT, Aviation)
3. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction activity that would block or limit access. (INDOT ESD)
4. Any work in a wetland area within right-of-way or in borrow/waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT ESD)
5. UNT 1 to Muddy Fork Sand Creek, Muddy Fork Sand Creek, and UNT 1 to Sand Creek will be labeled on the plans as "Do Not Disturb". (INDOT ESD)
6. Wetlands A, B, C, D, and the portion of Wetland E that will not be impacted will be labeled on the plans as "Do Not Disturb". (INDOT ESD)
7. If the scope of work or permanent or temporary right-of-way amounts change, INDOT ESD and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Seymour District)
8. Former Senior Center, 225 East Main Street (AI ID 6937), is located adjacent to the southwest of the western project area terminus for Des No. 1800256. According to the Site Status Letter issued by IDEM on January 23, 2008, sample results prior to remediation indicated soil and groundwater were impacted by TPH-GRO/ERO, VOCs, SVOCs, arsenic and lead. Current levels of contamination at the site meet current cleanup criteria; however, contamination is present off-site and potentially within the INDOT ROW. If excavation occurs in this area, it is likely that contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
9. Former Kocolene Service Station, 601 West Main Street (AI ID 2326), is located adjacent to the south of the eastern portion of the project area for Des No. 1800255. This was formally the site of a gas station. IDEM issued an NFA Determination Pursuant to RISC for the site on June 29, 2017. Low levels of soil and groundwater contamination remain on site. An ERC was recorded on the property by Decatur County on September 29, 2016. The ERC specifically prohibits the use or extraction of groundwater, and limits excavation activities. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Project Manager, Jeff Turley at [jturley@idem.in.gov](mailto:jturley@idem.in.gov), identified in the VFC documentation before further site activities occur. The project designer is responsible for completing this coordination if necessary. (INDOT SAM)
10. Ketchum's Shell, 106 South Michigan Avenue (AI ID 2294), is located adjacent to the southwest of the project area for Des No. 1800256. This is the site of an operating gas station. IDEM issued an NFA Determination Pursuant to RISC for the site on December 17, 2012. Low levels of soil and groundwater contamination remain on the site. An ERC was recorded on the property by Decatur County on September 4, 2012. The ERC specifically prohibits the use or extraction of groundwater at the site. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. Coordination will be conducted with the IDEM Project Manager, Kay Ifekoya ([kifekoya@idem.in.gov](mailto:kifekoya@idem.in.gov)) identified in the VFC documentation before further site activities occur. The project designer is responsible for completing this coordination if necessary. (INDOT SAM)
11. Green Barn USA, SR 46 and Sunset Drive (AI ID 4640), is located adjacent to the north of the central portion of the project area for Des No. 1800255. According to the October 17, 1991 Notification of Underground Storage Tanks, one (1) 3,000-gallon UST of unknown age was taken out of service in October 1991. No other information regarding the current status of the tank was found in the IDEM VFC. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
12. Kiel Brothers Oil Bulk Plant, 1010 West Main Street (AI ID 3415), is mapped adjacent to the south of the central portion of the project area for Des No. 1800255. No information was found in the IDEM VFC for this UST site. The current status of the UST is unknown; therefore, in addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
13. Muncie Bread Company (Caldron Brothers Vending Incorporated) 117 South Anderson Street (AI ID 2828), is located approximately 0.02 miles south of the eastern portion of the project area for Des No. 1800255. According to the September 4, 1990 Notification of Underground Storage Tanks, one (1) 550-gallon UST was taken out of service in August 1990. The tank

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was removed from the ground; however, it does not appear as though confirmatory soil or groundwater samples were taken during UST removal. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)

14. Scripture (Garry Hadler) CPD Incorporated (also listed as Water Tower Center), 325 West Main Street (AI ID 1768), is located adjacent to the southeast of the eastern project area terminus for Des No. 1800255. The UST was emptied and has not been in use since 1983. In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
15. Paul S Walston - 302 West Main Street (AI ID 3590), is located approximately 0.02 mile north east to the eastern project area terminus for Des No. 1800255. The UST site was a filling station dating back to 1935. The USTs have not been in use since 1971 and were abandoned in place (Filled with inert material). In addition to petroleum contamination, it is likely that lead would be in the soil/groundwater. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. (INDOT SAM)
16. Delta Faucet Company, 1425 West Main Street (AI ID 15043), is located adjacent to the south of the central portion of the project area for Des No. 1800255. Coordination with IDEM will occur. The project designer is responsible to complete this coordination if necessary (INDOT SAM)
17. Irvington Materials Incorporated, 1100 West Main Street (AI ID 15586), is located adjacent to the north of the central portion of the project area for Des No. 1800255. Coordination with IDEM will occur. The project designer is responsible to complete this coordination if necessary (INDOT SAM)
18. One petroleum well IGS# 134956 is in the project area just south of 46 about 40' and about 120' west of the railroad tracks. There is no real data known on this well and the location is approximate based on an old map. The IDNR inspector could not locate the well on March 7, 2016 and it was presumed to be plugged. However, if this well is encountered during this project it will need to be plugged properly. (IDNR Division of Oil and Gas)
19. The limestone curbs within the project area will be carefully removed by the contractor and curbs in good condition will be provided to the City of Greensburg for storage and reuse at other locations in the following priority: within the Greensburg Eastside Residential Historic District, in other historic districts within Greensburg, or other public areas within Greensburg. The City of Greensburg shall make a good faith effort to reuse as many of the limestone curbs as possible within ten (10) years of execution of this agreement. (FHWA and Indiana SHPO)
20. The INDOT shall reinstall the salvaged limestone curbs in good condition in front of the Charles Zoller House (approximately 260 linear feet) and in front of the Wilderwood House (approximately 180 linear feet). The INDOT shall submit detailed curb construction plans to the Indiana SHPO and other consulting parties for a 30-day review and comment period before construction. If the Indiana SHPO or other consulting parties do not respond within thirty (30) days, acceptance shall be assumed. If the Indiana SHPO or other consulting parties respond with recommendations, a good faith effort to accommodate the recommendations shall be made. FHWA shall inform the Indiana SHPO and other consulting parties of its response to such comments. (FHWA and Indiana SHPO)
21. The INDOT shall construct the multi-use path to be 8-feet wide and with concrete within the Greensburg Eastside Residential Historic District. The back of the new multi-use path shall be no closer to the homes within the district than is the back of the existing sidewalk. (FHWA and Indiana SHPO)
22. The INDOT shall replicate the existing stamped street names ("Stewart St" and "Main St") in the multi-use path concrete at the northeast quadrant of that intersection. (FHWA and Indiana SHPO)
23. The INDOT shall avoid damage to any stone retaining walls, steps, sidewalks or walkways in front of the Charles Zoller House or within the Greensburg Eastside Residential Historic District. If damage occurs, INDOT shall reconstruct the damaged area in-kind or as close as possible to the Secretary of the Interior's Standards for Reconstruction. (FHWA and Indiana SHPO)
24. The INDOT shall plant appropriate varieties of street trees in the grass buffer (between the curb and multi-use path) within the Greensburg Eastside Residential Historic District. The INDOT shall submit detailed planting plans to the Indiana SHPO and other consulting parties for a 30-day review and comment period before construction. If the Indiana SHPO or other consulting parties do not respond within thirty (30) days, acceptance shall be assumed. If the Indiana SHPO or other consulting parties respond with recommendations, a good faith effort to accommodate the recommendations shall be made. FHWA shall inform the Indiana SHPO and other consulting parties of its response to such comments. (FHWA and Indiana SHPO)
25. The INDOT shall submit detailed street lighting plans (adjacent to and within the Charles Zoller House and Greensburg Eastside Residential Historic District) to the Indiana SHPO and other consulting parties for a 30-day review and comment period before construction. If the Indiana SHPO or other consulting parties do not respond within thirty (30) days, acceptance shall be assumed. If the Indiana SHPO or other consulting parties respond with recommendations, a good faith effort to accommodate the recommendations shall be made. FHWA shall inform the Indiana SHPO and other consulting parties of its response to such comments. (FHWA and Indiana SHPO)

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26. A Unique Special Provision (USP) will be developed for the contractor to notify all emergency and school officials prior to any construction that would limit or block access. An additional USP will be developed for the contractor to maintain access to all properties at all times during construction. (INDOT, ESD)
27. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
28. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
29. Lighting AMM2: When installing new or replacing existing permanent lights, use downward facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
30. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
31. Tree Removal AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present (October 1 through March 31), or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS and IDNR-DFW)
32. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
33. Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)

**For Further Consideration:**

1. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. (IDNR DFW)
2. The Division of Fish & Wildlife recommends avoiding removing urban trees to the greatest extent possible and replacing trees that must be removed. (IDNR DFW)

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## SR 46: Pavement Rehabilitation– Decatur County, Indiana

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### Appendix A: INDOT Supporting Documentation

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### Appendix B: Graphics

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INDOT District USFWS Bat Database Check Email .....	C30
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State Historic Preservation Officer (SHPO) 800.11 Documentation Concurrence .....	D294
ACHP Response Letter to “Adverse Effect” Finding .....	D297
Miami Tribe of Oklahoma Response Letter to “Adverse Effect” Finding .....	D298
Memorandum of Agreement (MOA) Signature Request Email .....	D299
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Fully Executed MOA Distribution Letter .....	D311

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## SR 46: Pavement Rehabilitation– Decatur County, Indiana

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### Appendix F: Water Resources

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### Appendix H: Air Quality

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Greensburg Bicycle and Pedestrian System Plan ( <i>applicable pages only</i> ) .....	I34

**Categorical Exclusion**

**Appendix A**

**INDOT Supporting Documentation**

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations<sup>6</sup></b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat) *</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>7</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>8</sup>
<b>Threatened/Endangered Species (Any other species) *</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>9</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>10</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>11</sup>
<b>Approval Level</b> <ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

<sup>7</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>8</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

<sup>9</sup> Potential for causing a disproportionately high and adverse impact.

<sup>10</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

<sup>11</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

**Categorical Exclusion**  
**Appendix B**  
**Graphics**

Project Location  
SR 46 Pavement Improvement Project  
Des. Numbers 1800255 & 1800256  
Decatur County, Indiana



Des No. 1800255

Des No. 1800256

**Legend**

 Project

Map Datum: NAD 83  
Map Projection: UTM Zone 16 North  
This map is intended to serve as an aid in graphic representation only.  
This information is not warranted for accuracy or other purposes.  
Data obtained from the State of Indiana GIO Library.  
Orthophotography obtained from Indiana Map Framework Data.

Project Area

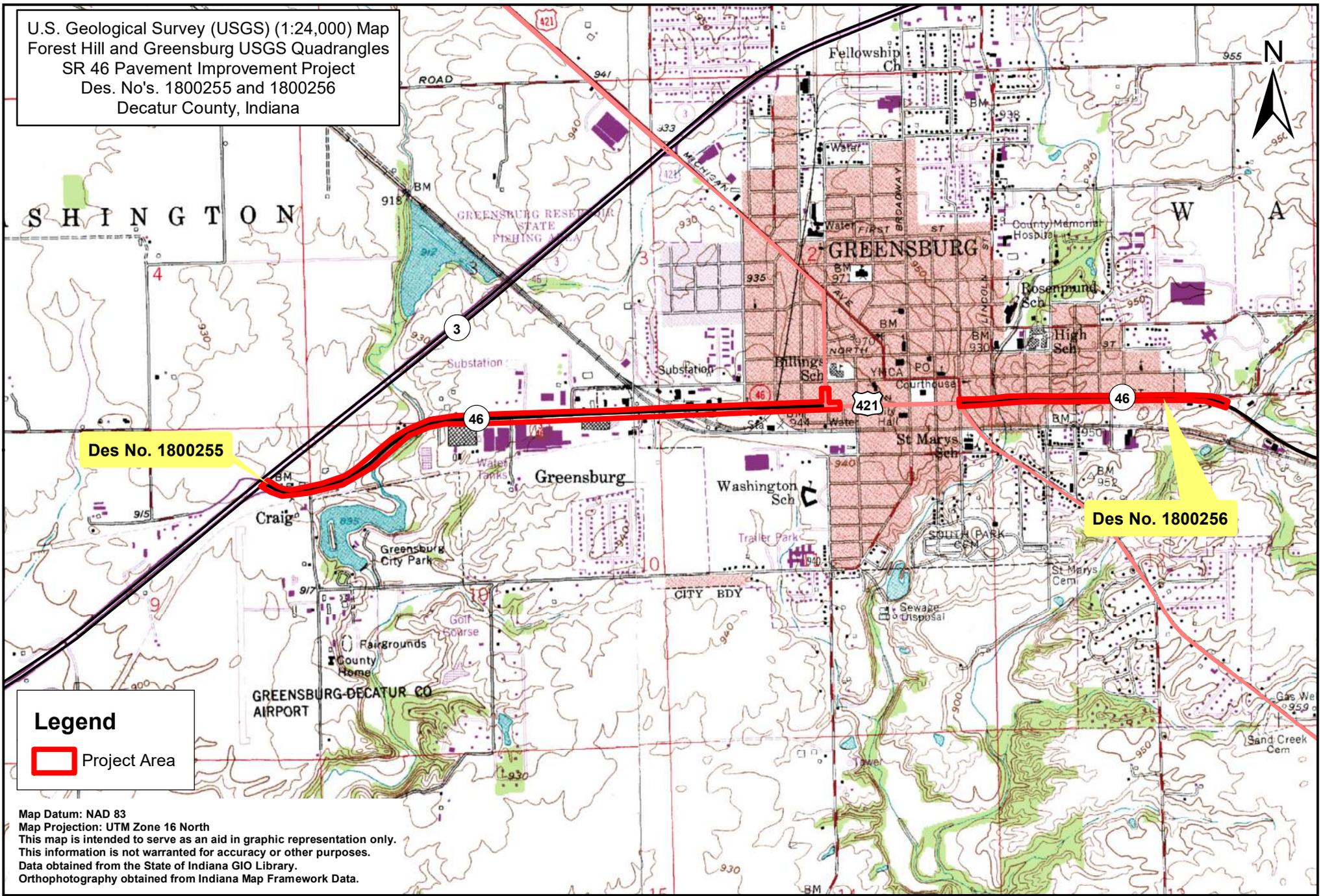


**Project Location Map**

Miles

Location: SR 46  
Township: Washington  
County: Decatur

U.S. Geological Survey (USGS) (1:24,000) Map  
 Forest Hill and Greensburg USGS Quadrangles  
 SR 46 Pavement Improvement Project  
 Des. No's. 1800255 and 1800256  
 Decatur County, Indiana



**Legend**

 Project Area

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

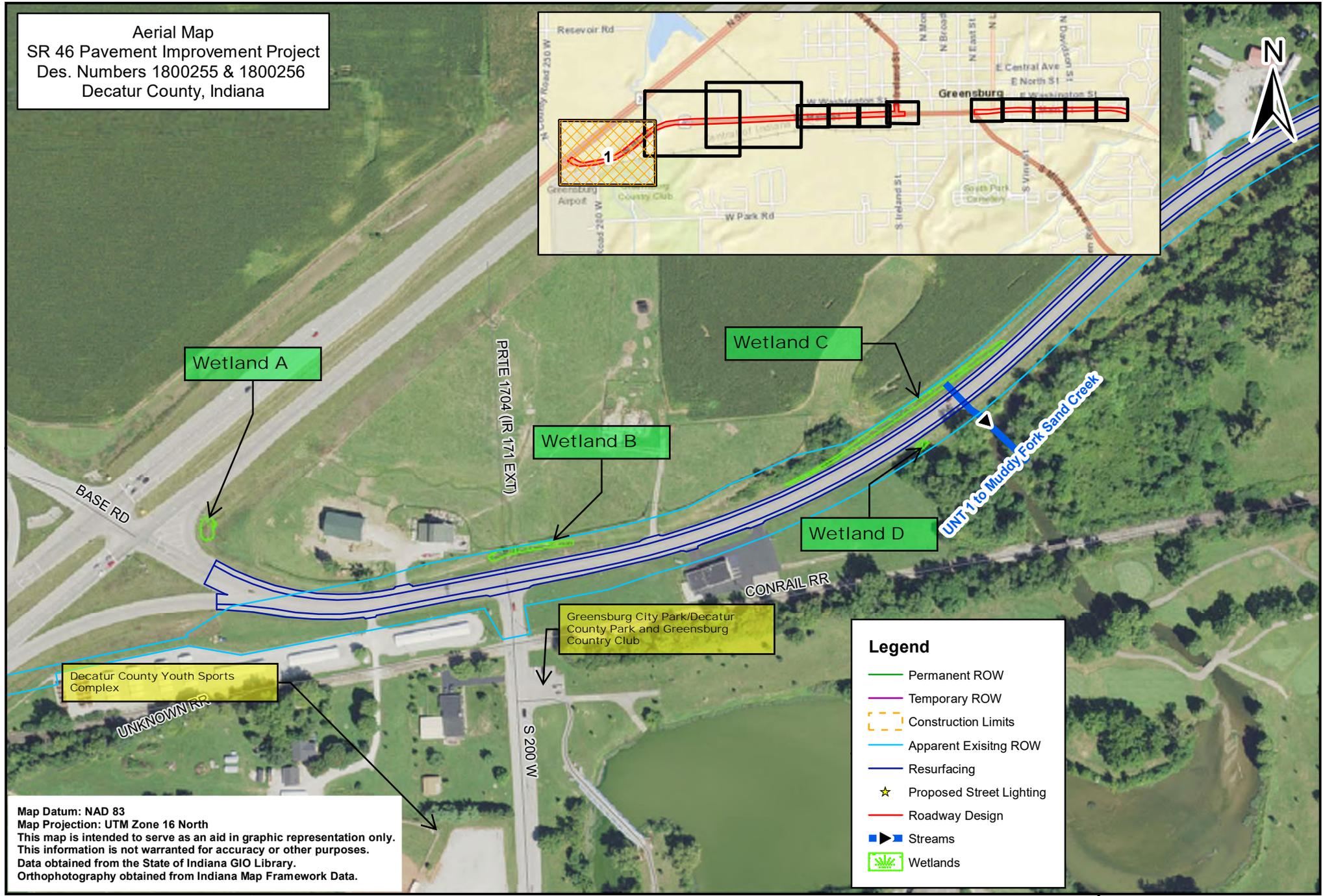
**USGS Topographic Map**



**Location: SR 46**  
**Township: Washington**  
**County: Decatur**



Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

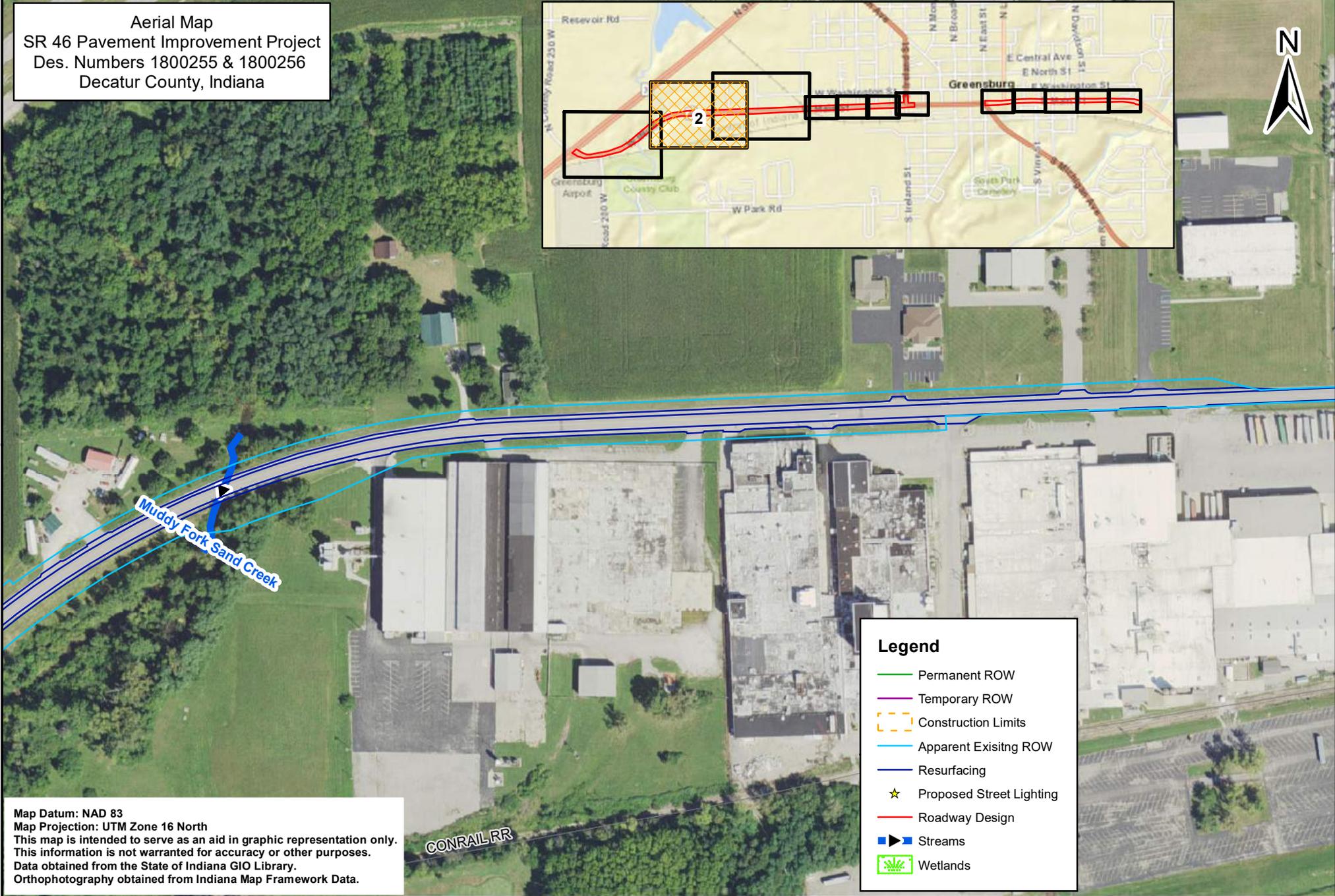


Aerial Map 1 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

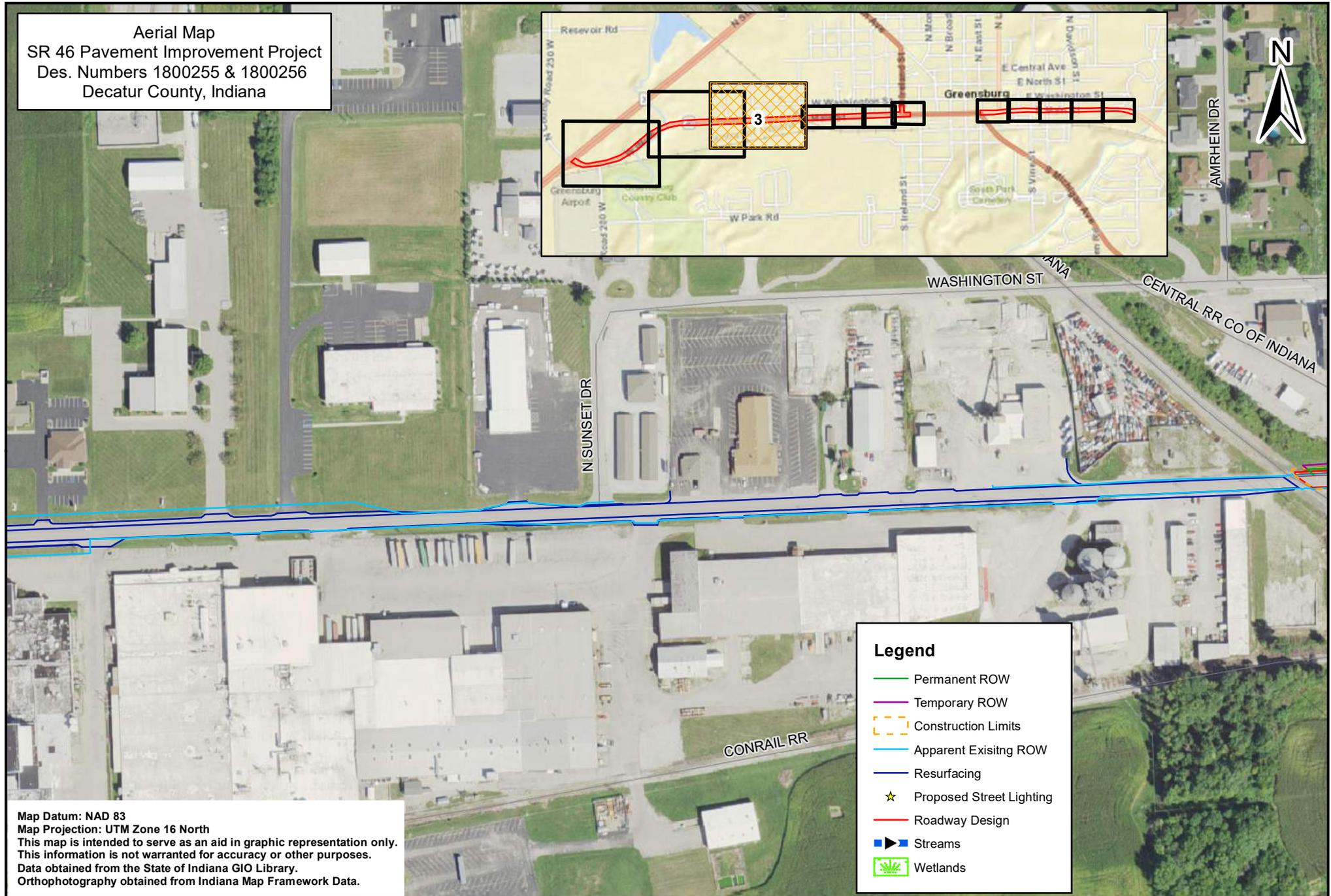


Aerial Map 2 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



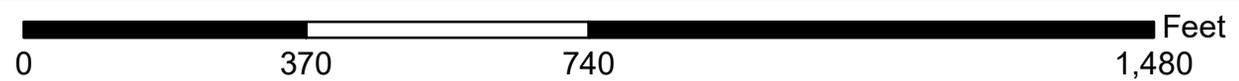
**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.



Aerial Map 3 of 12

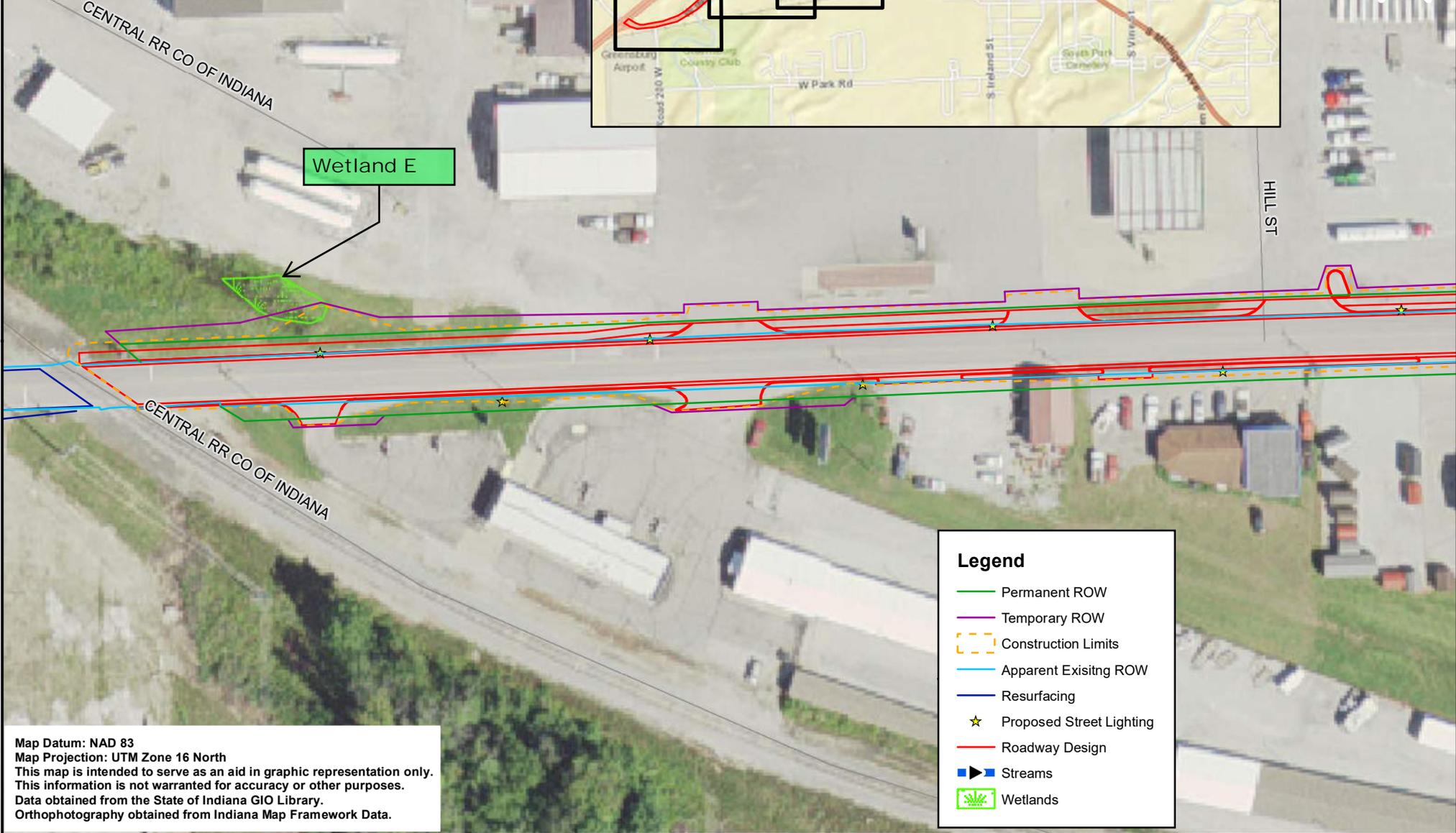


Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Wetland E



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.



Aerial Map 4 of 12

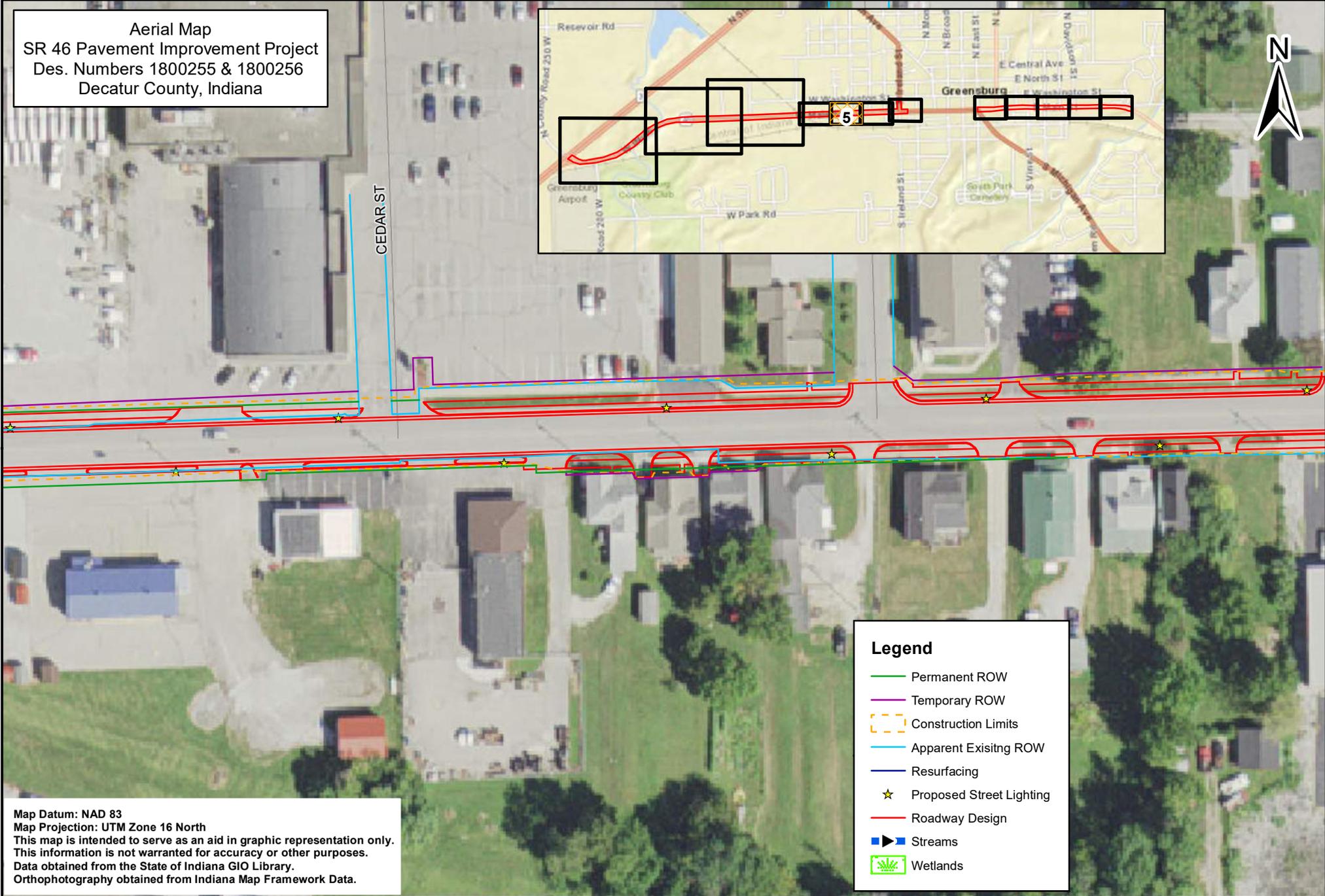


Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



CEDAR ST



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

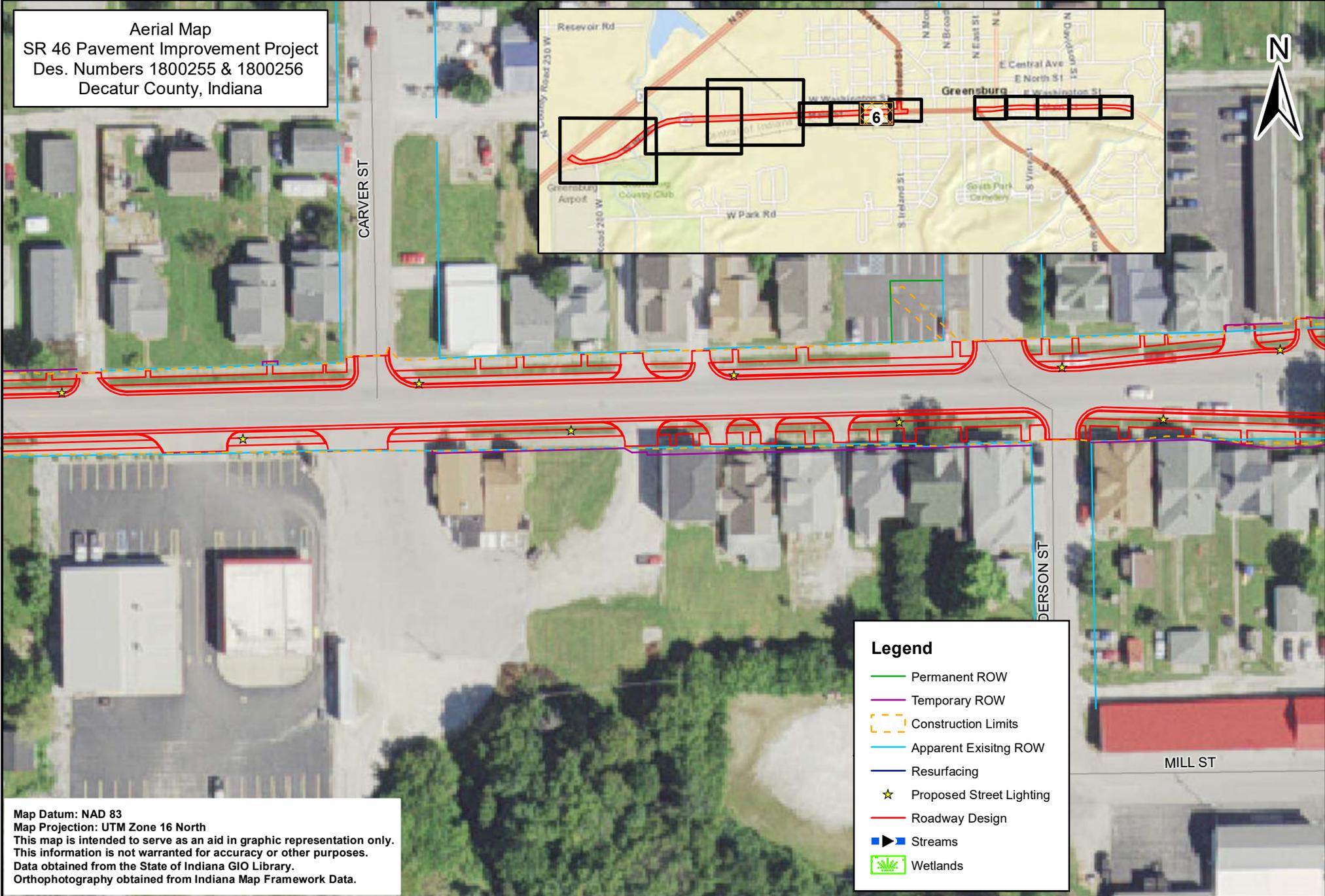


Aerial Map 5 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

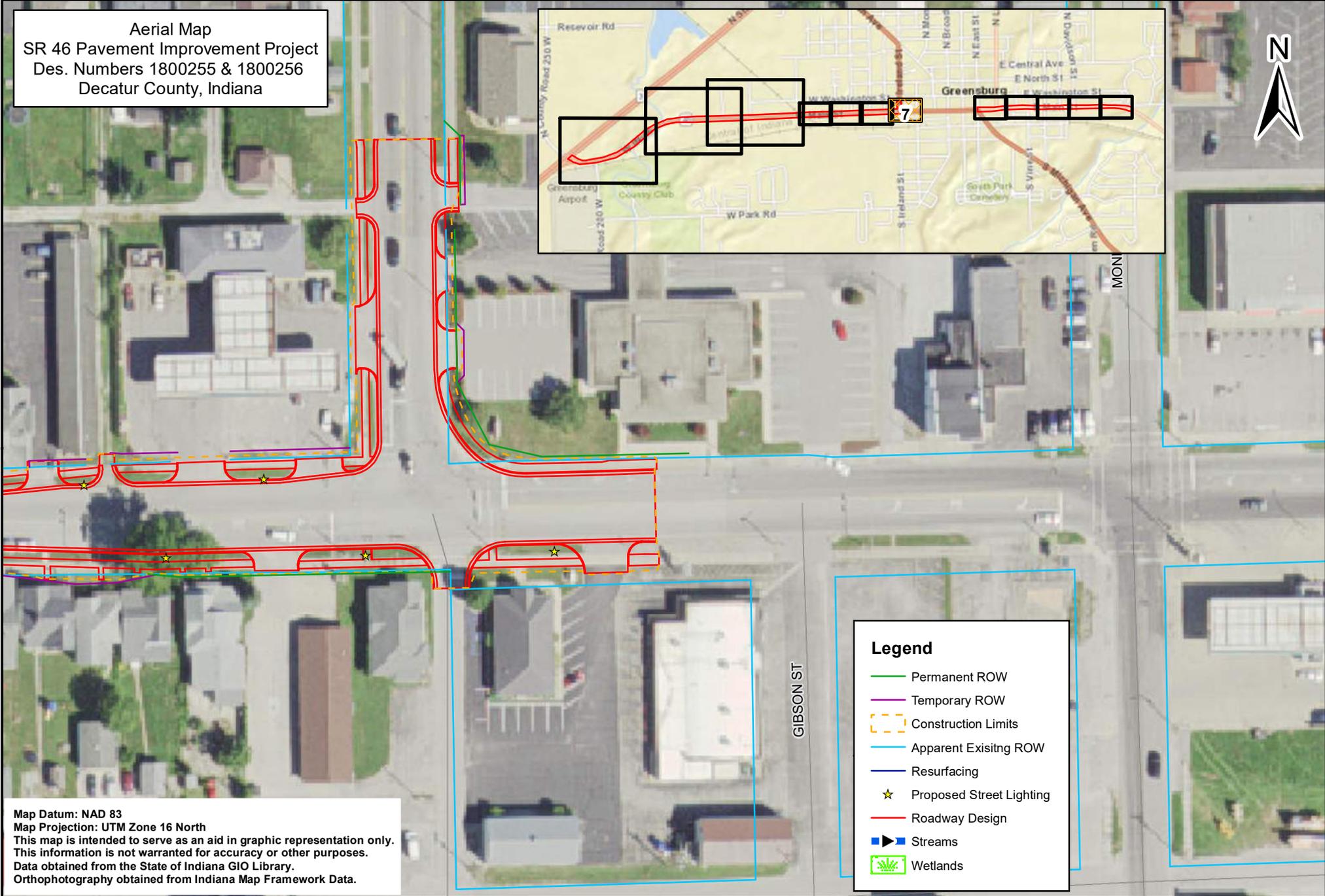
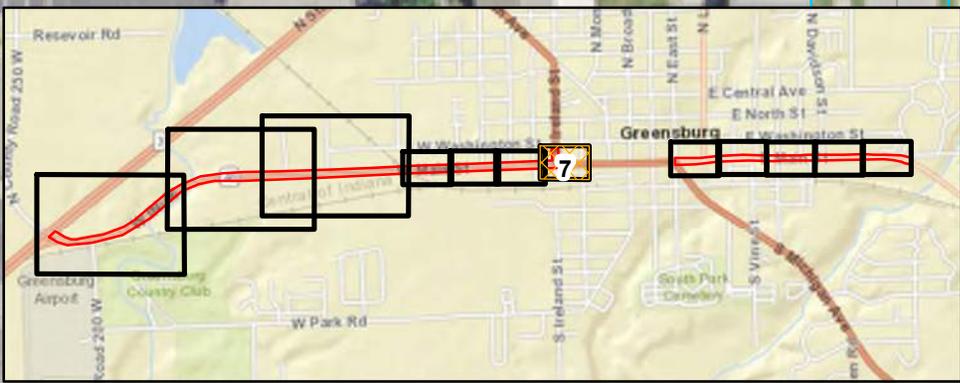


Aerial Map 6 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
 This information is not warranted for accuracy or other purposes.  
 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

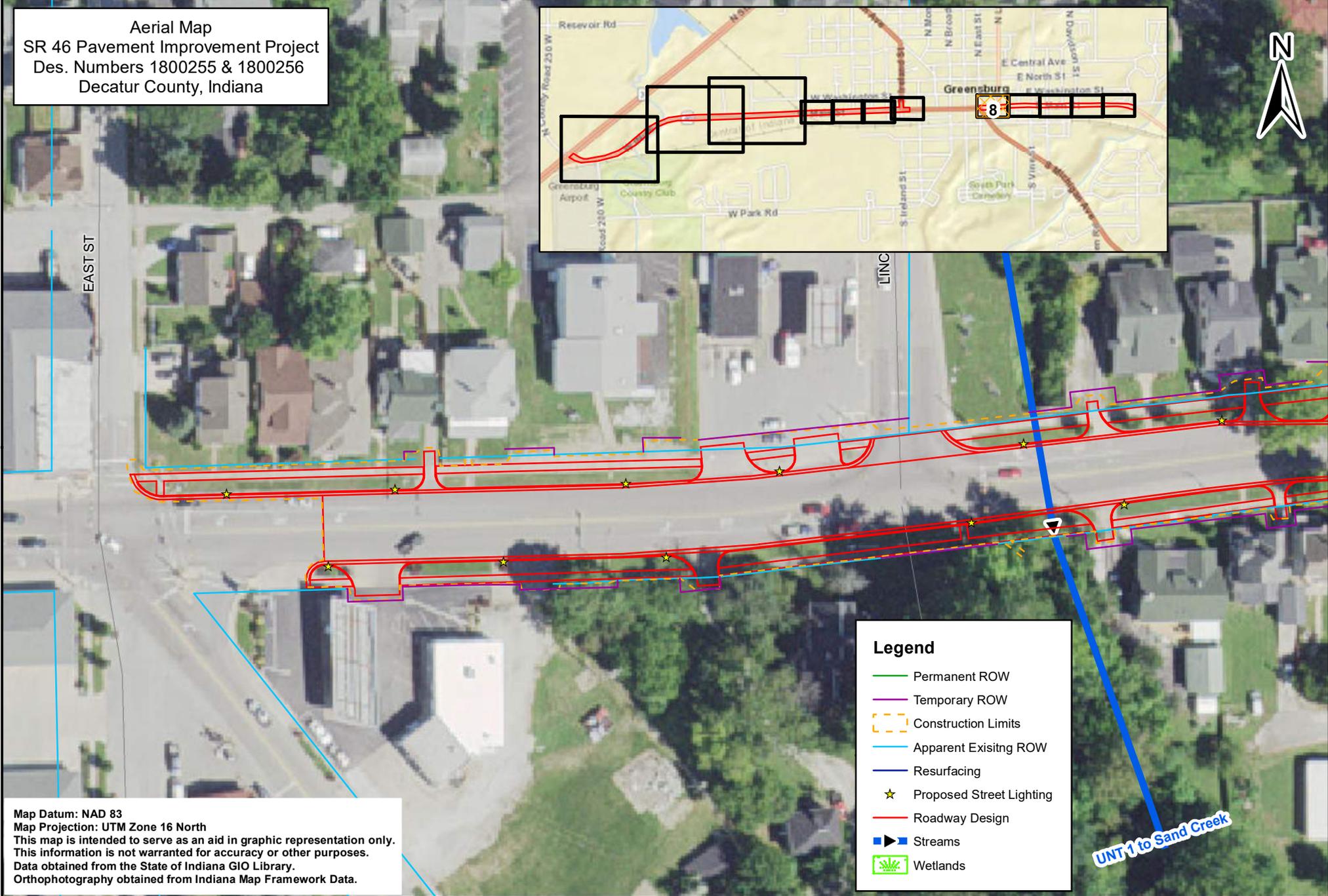
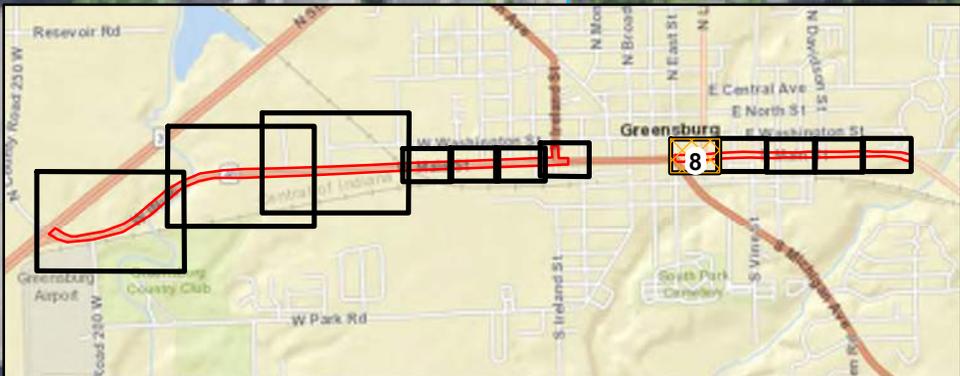


Aerial Map 7 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
 This information is not warranted for accuracy or other purposes.  
 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands

UNT 1 to Sand Creek

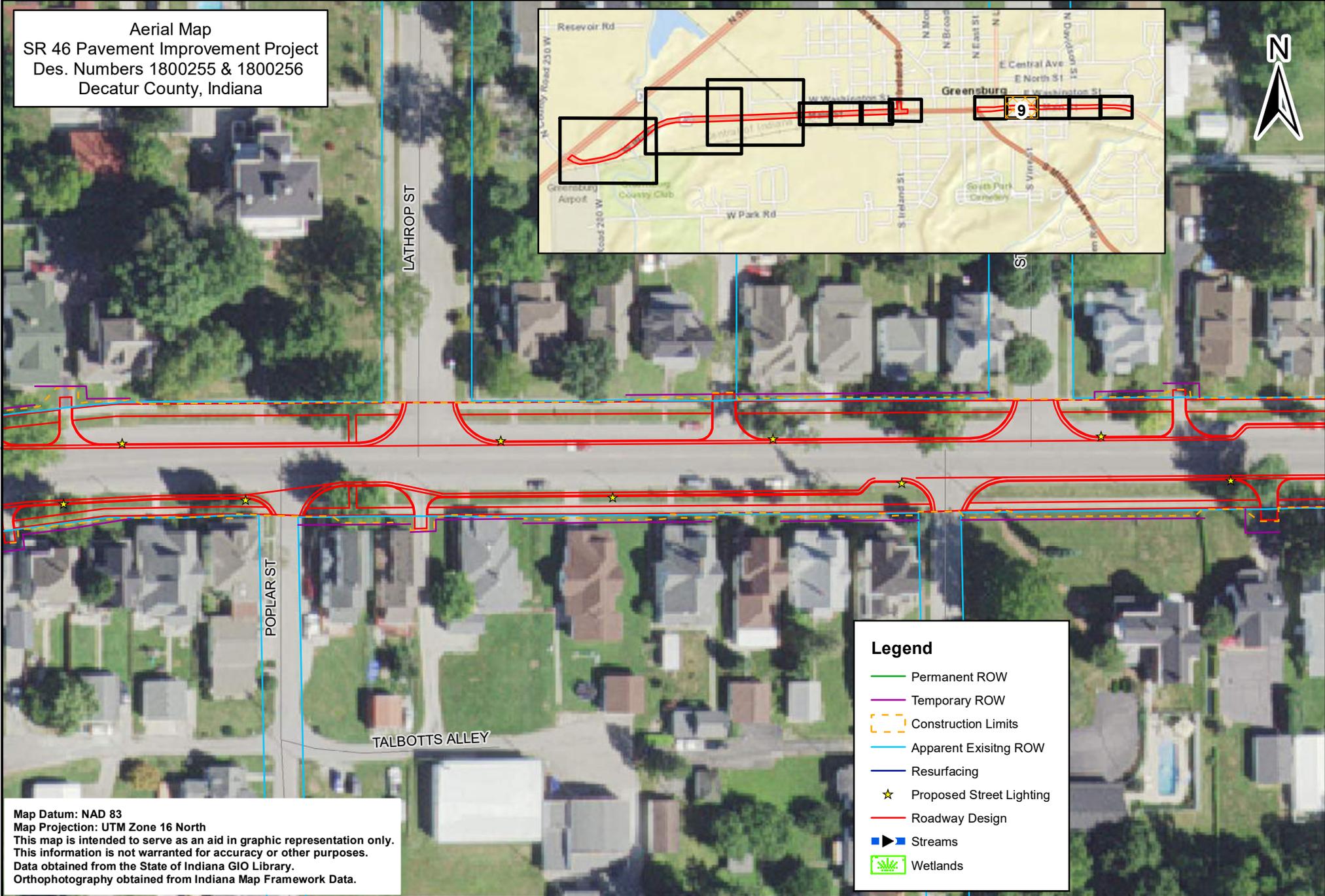


Aerial Map 8 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

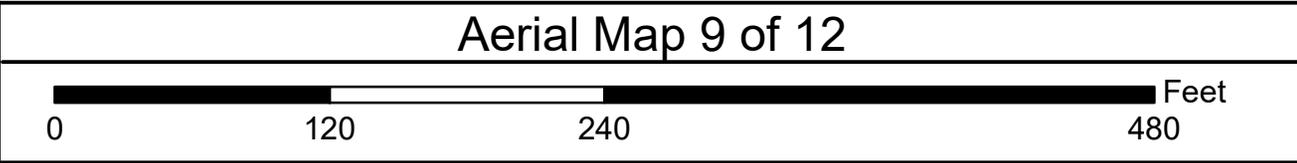
Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

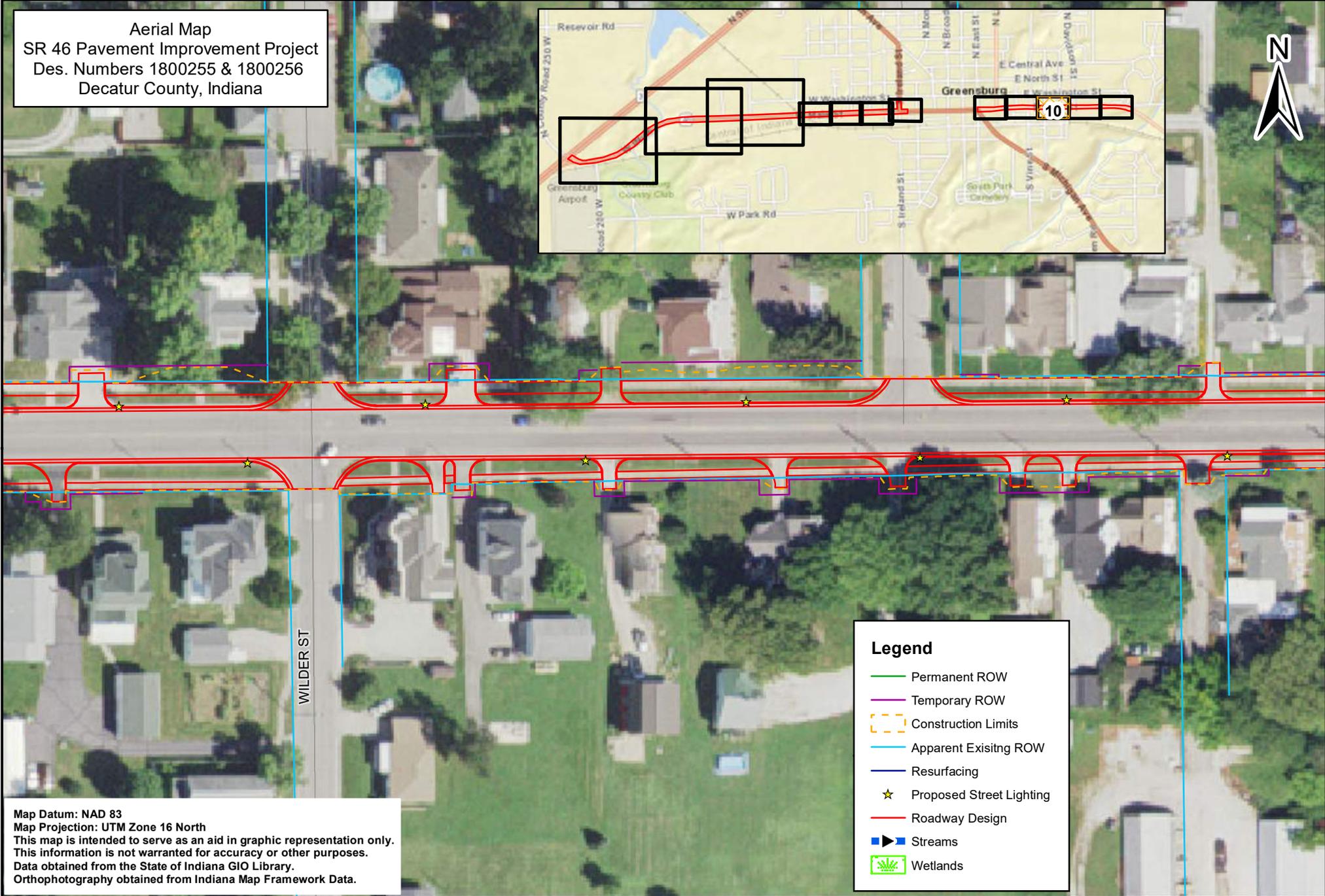
**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶ Streams
- Wetlands



**Location: SR 46**  
**Township: Washington**  
**County: Decatur**

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Permanent ROW
- Temporary ROW
- - - Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶▶ Streams
- ▨ Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

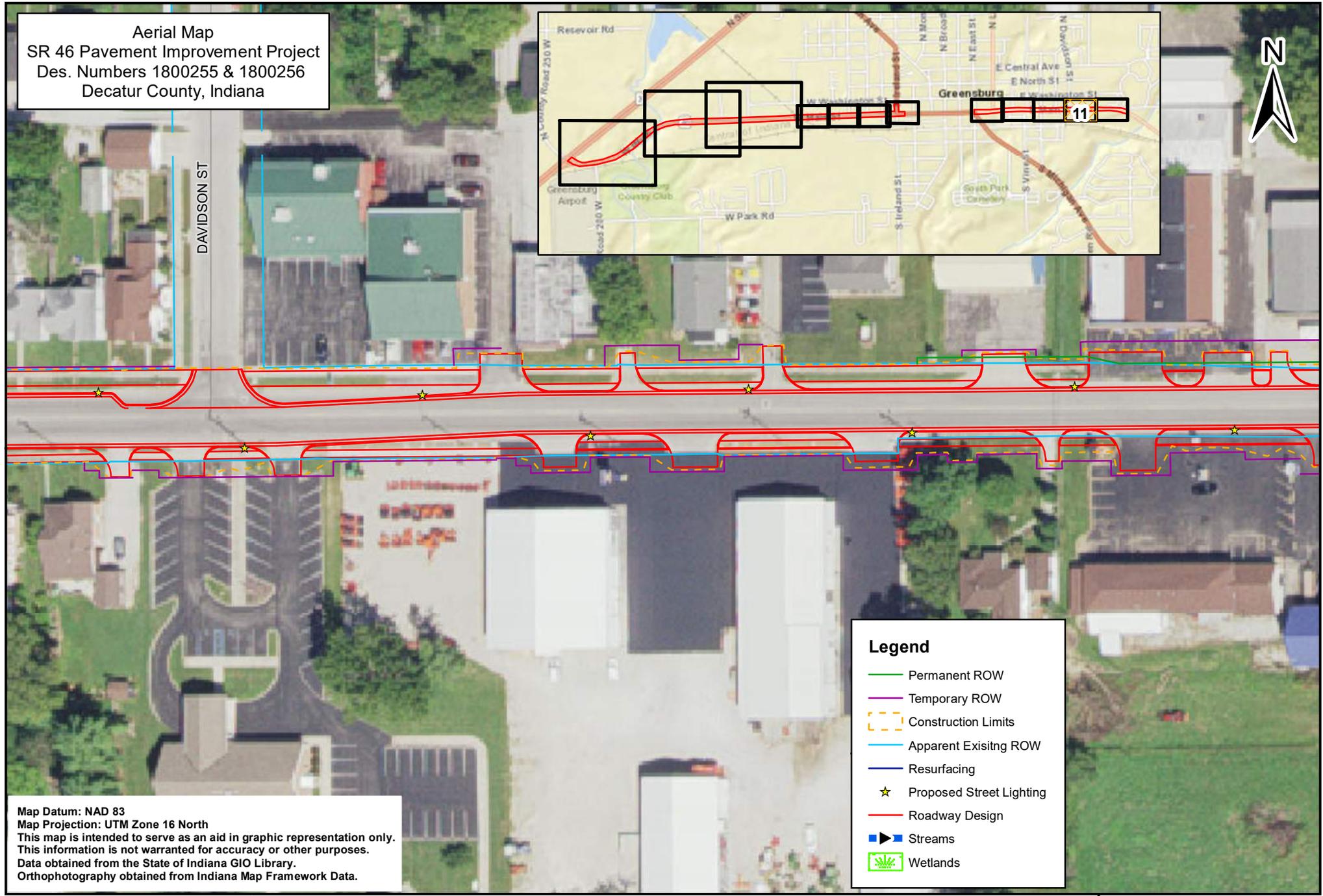


Aerial Map 10 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

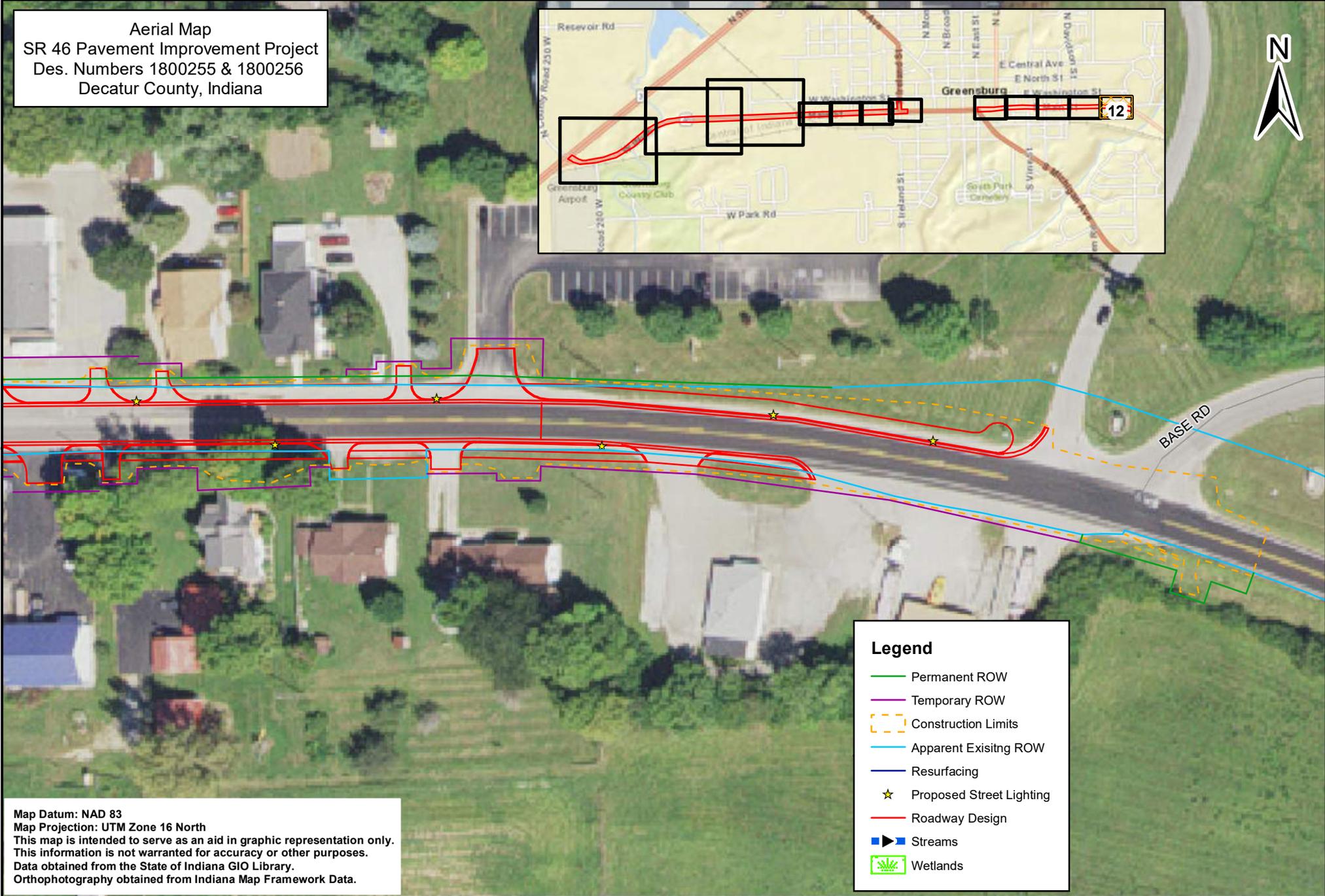


Aerial Map 11 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

Aerial Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Permanent ROW
- Temporary ROW
- Construction Limits
- Apparent Existing ROW
- Resurfacing
- ★ Proposed Street Lighting
- Roadway Design
- ▶▶ Streams
- Wetlands

Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Orthophotography obtained from Indiana Map Framework Data.

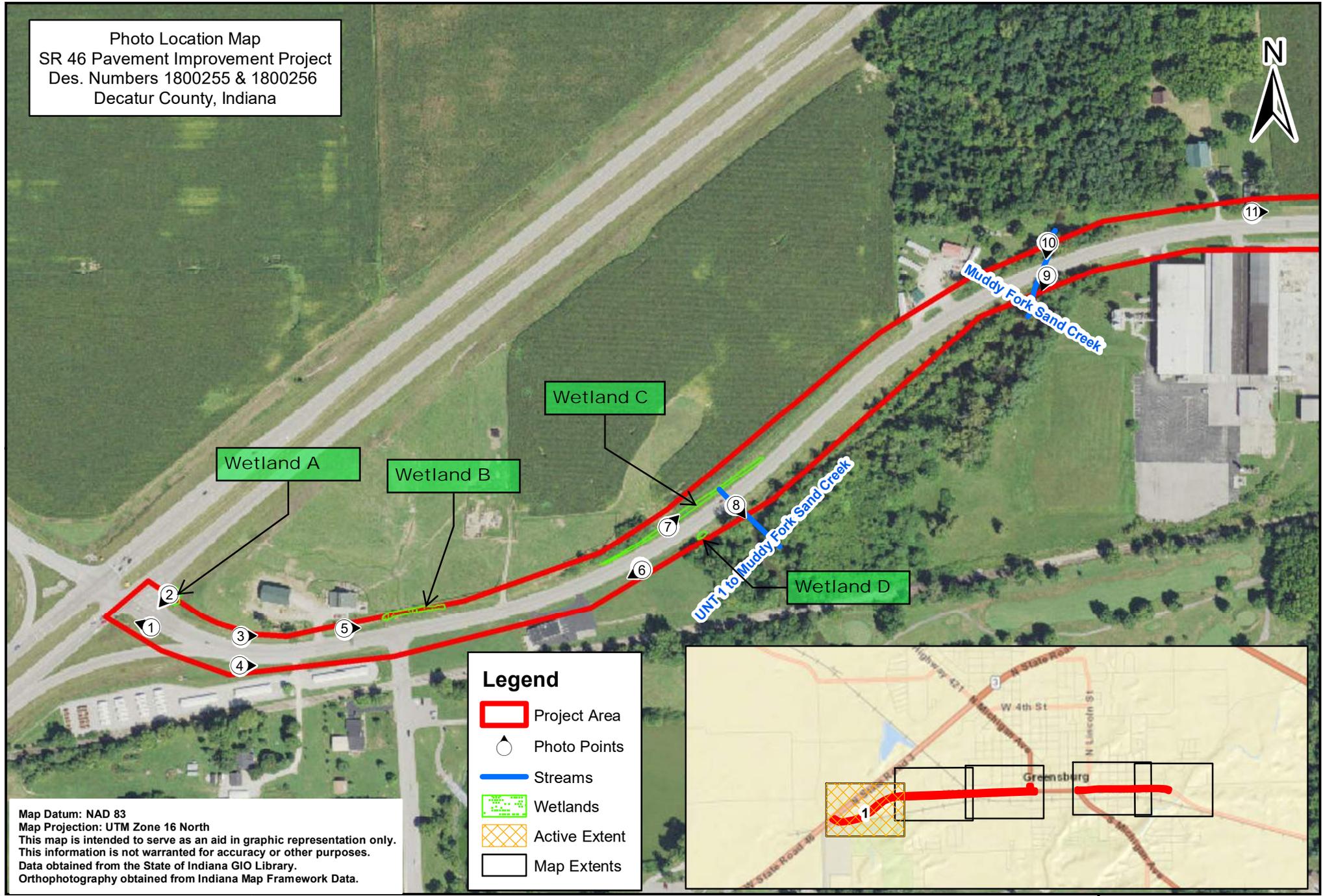


Aerial Map 12 of 12



Location: SR 46  
 Township: Washington  
 County: Decatur

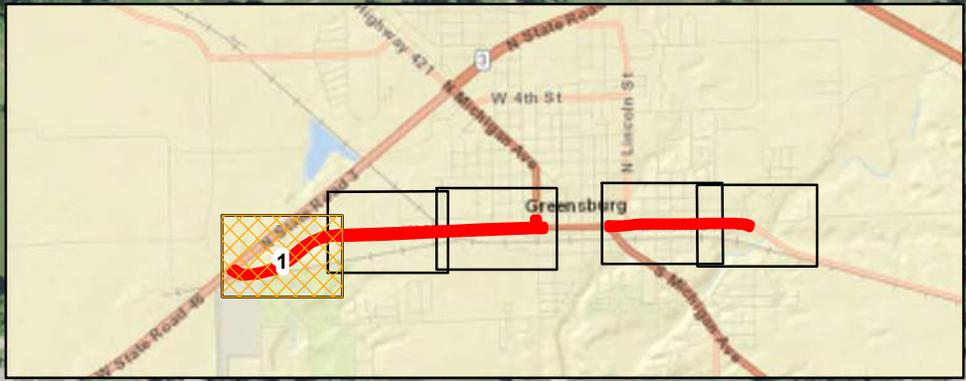
Photo Location Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
 This map is intended to serve as an aid in graphic representation only.  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

**Legend**

- Project Area
- Photo Points
- Streams
- Wetlands
- Active Extent
- Map Extents

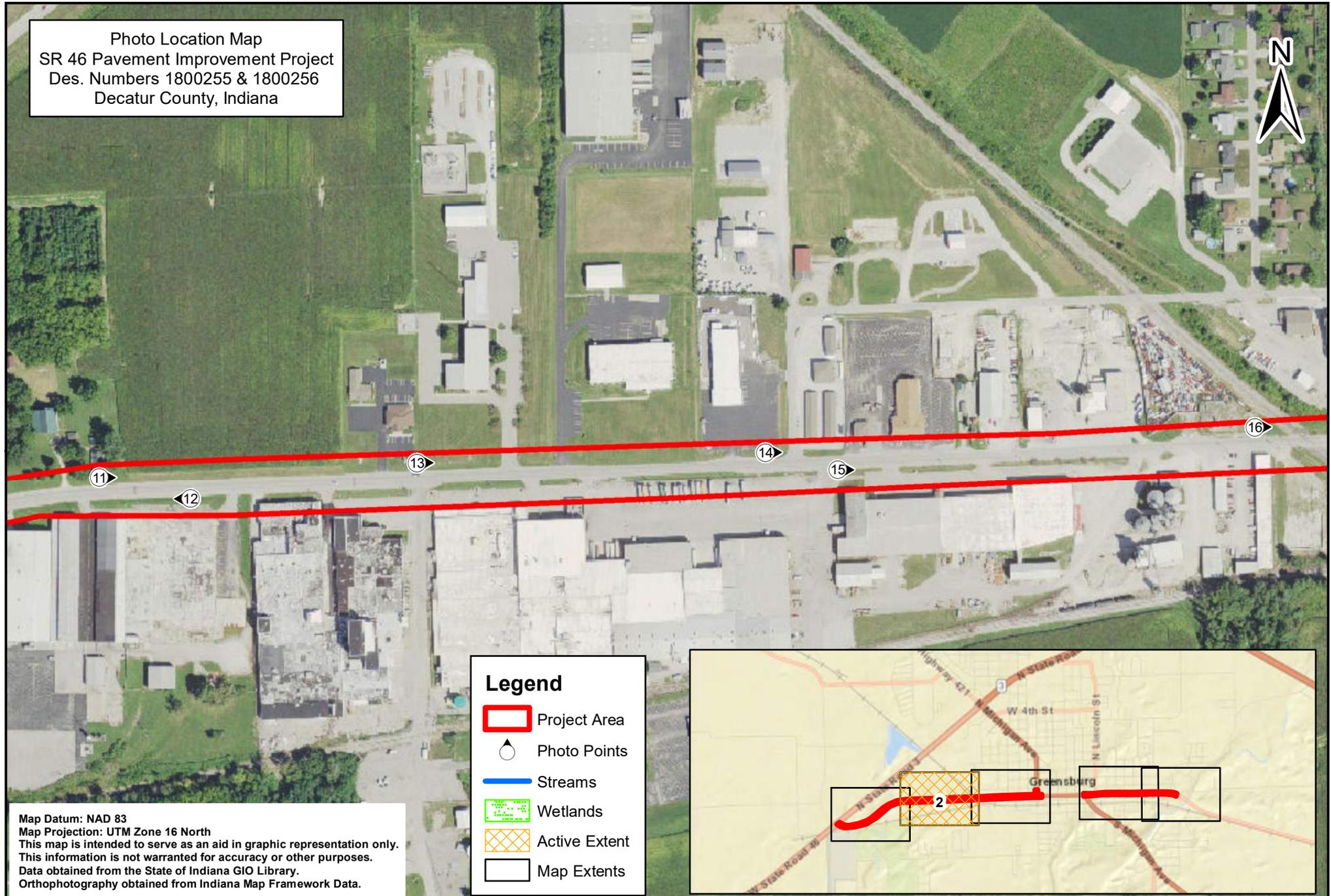


**Photo Location Map 1 of 5**

0                      500                      1,000                      2,000 Feet

**Location: SR 46**  
**Township: Washington**  
**County: Decatur**

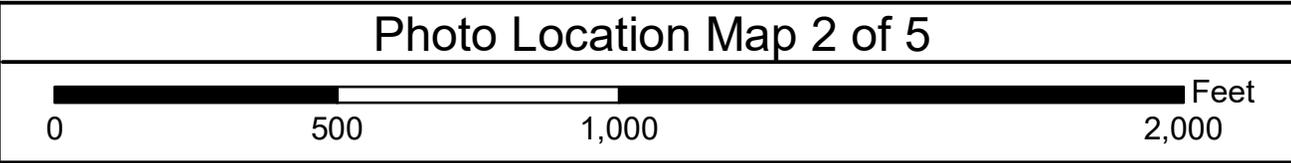
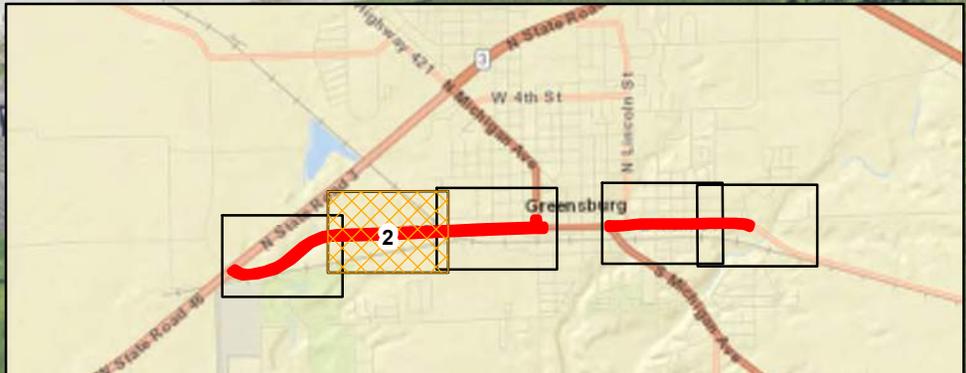
Photo Location Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

**Legend**

- Project Area
- Photo Points
- Streams
- Wetlands
- Active Extent
- Map Extents



**Location: SR 46**  
**Township: Washington**  
**County: Decatur**

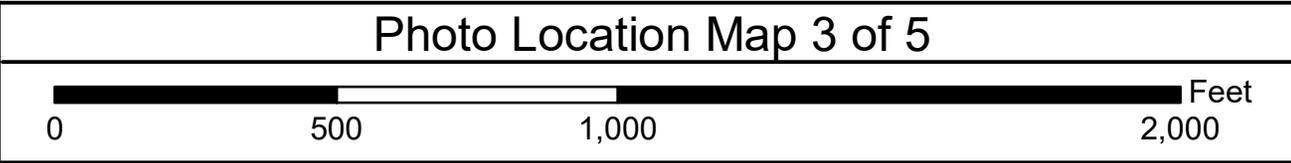
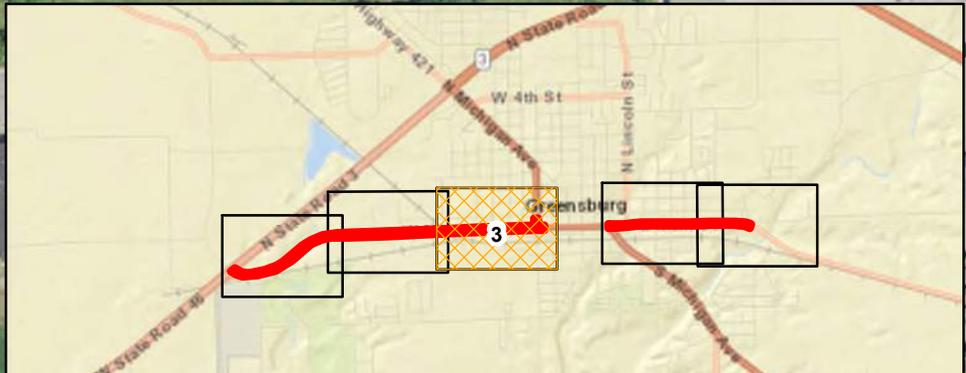
Photo Location Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

- Project Area
- Photo Points
- Streams
- Wetlands
- Active Extent
- Map Extents

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**Location: SR 46**  
**Township: Washington**  
**County: Decatur**

Photo Location Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



**Legend**

-  Project Area
-  Photo Points
-  Streams
-  Wetlands
-  Active Extent
-  Map Extents

Map Datum: NAD 83  
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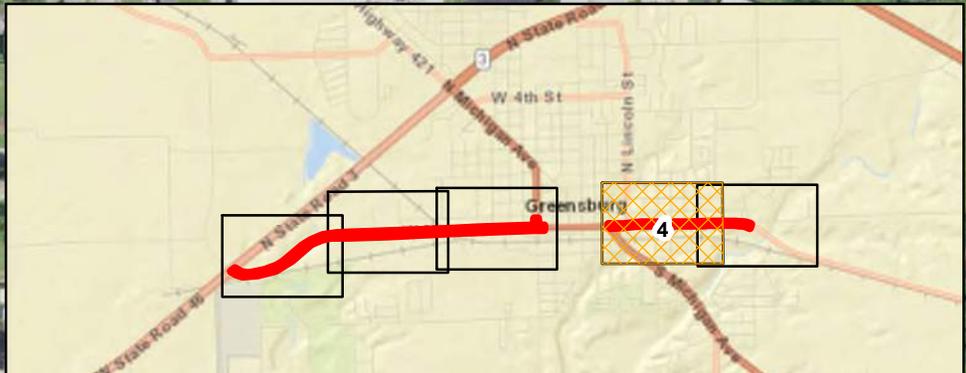
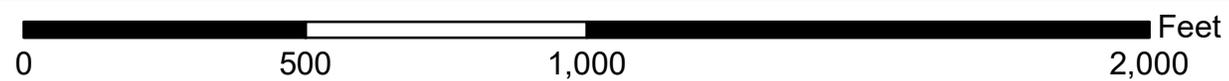
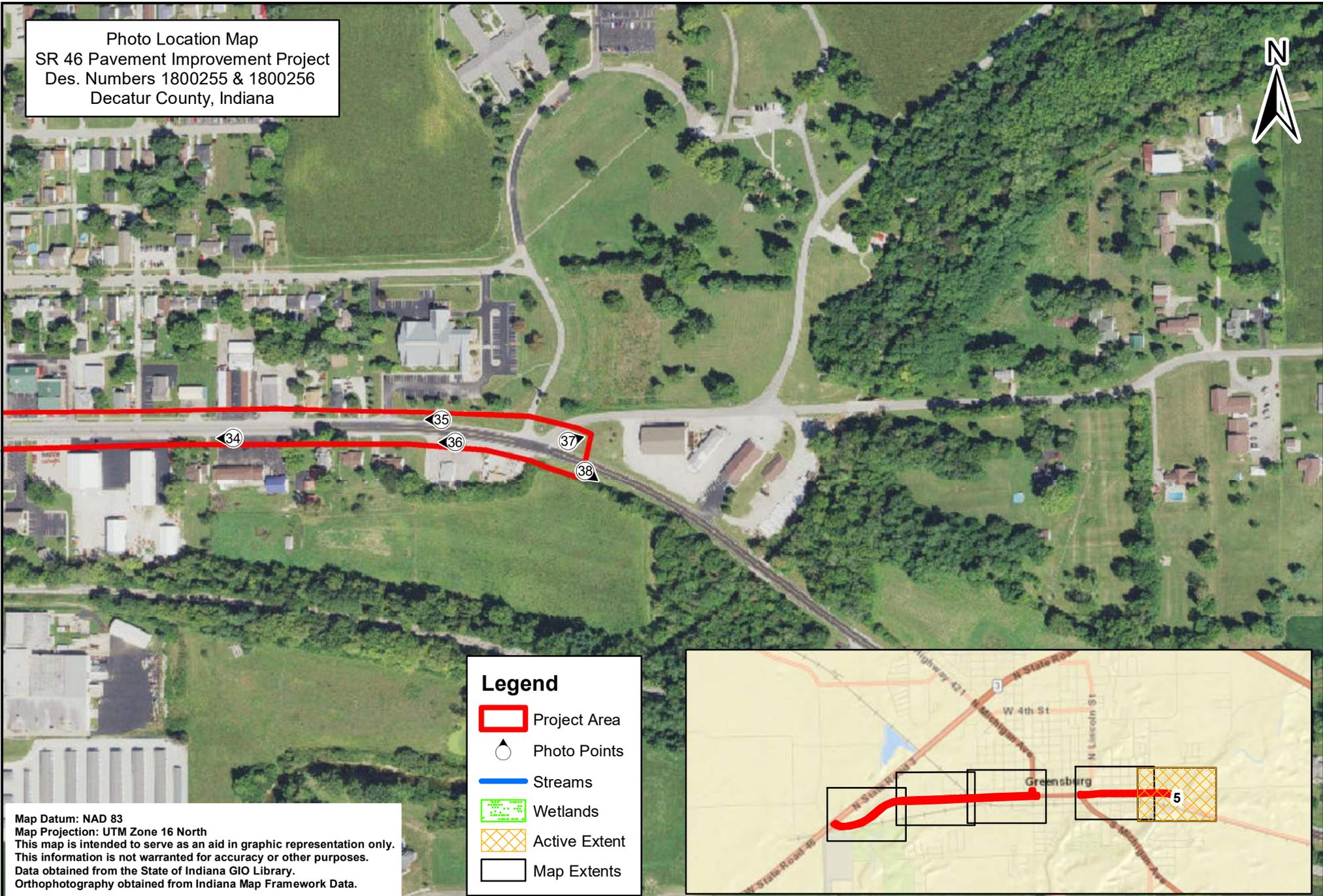


Photo Location Map 4 of 5



Location: SR 46  
 Township: Washington  
 County: Decatur

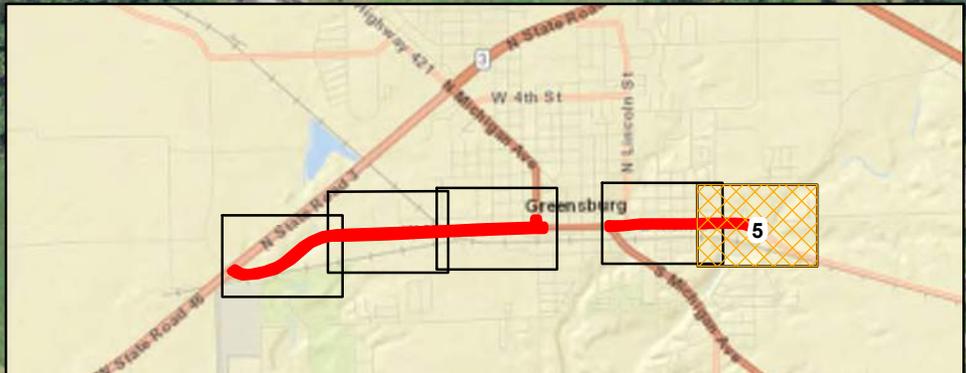
Photo Location Map  
 SR 46 Pavement Improvement Project  
 Des. Numbers 1800255 & 1800256  
 Decatur County, Indiana



Map Datum: NAD 83  
 Map Projection: UTM Zone 16 North  
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 Data obtained from the State of Indiana GIO Library.  
 Orthophotography obtained from Indiana Map Framework Data.

**Legend**

-  Project Area
-  Photo Points
-  Streams
-  Wetlands
-  Active Extent
-  Map Extents



**Photo Location Map 5 of 5**

0                      500                      1,000                      2,000 Feet

**Location: SR 46**  
**Township: Washington**  
**County: Decatur**



1. Facing northwest towards the northwest end of the project area (Des No. 1800255) looking at SR 3 and SR 46 intersection.



2. Facing southwest looking at a wetland located on the north side of the intersection of SR 3 and SR 46.



3. Facing east from the SR 46 westbound lane looking at the surrounding landscape.



4. Facing east from the SR 46 eastbound lane looking at the surrounding landscape.



5. Facing east along the SR 46 westbound lane looking at a concrete lined ditch and at a wetland found within the project area.



6. Facing southwest along the SR 46 eastbound lane and at the surrounding landscape.



7. Facing northeast along the SR 46 westbound lane looking at a wetland confined to a roadside ditch.



8. Facing southeast looking downstream at an unnamed tributary (UNT 1 to Muddy Fork Sand Creek) which is located along the eastbound lane of SR 46.



9. Facing southwest at Muddy Fork Sand Creek from Bridge No. 046-16-00945 that carries SR 46 over Muddy Fork Sand Creek.



10. Facing south looking downstream at Muddy Fork Sand Creek and at Bridge No. 046-16-00945.



11. Facing east along the SR 46 westbound lane looking at the surrounding landscape.



12. Facing east along SR 46 eastbound lane looking at the surrounding landscape.



13. Facing east along the SR 46 westbound lane looking at the surrounding landscape.



14. Facing east along the SR 46 westbound lane looking at the surrounding landscape and towards the Sunset Dr. and SR 46 intersection.



15. Facing east along the SR 46 eastbound lane looking at the surrounding landscape.



16. Facing east along the SR 46 westbound lane looking at the surrounding landscape and at the Central Railroad Company of Indiana (CIND) railroad crossing.



17. Facing west looking at a wetland located along the westbound lane of SR 46. Note the CIND railroad crossing in the background.



18. Facing west along the SR 46 eastbound lane looking at the surrounding landscape.



19. Facing east along the SR 46 eastbound lane looking at the surrounding landscape and sidewalk.



20. Facing east looking at the intersection of SR 46 and West St. along the westbound lane of SR 46.



21. Facing west along the SR 46 westbound lane looking at the surrounding landscape and down the sidewalk. Note speedway gas station to the right of photo.



22. Facing north from SR 46 and N. Ireland St. intersection looking up the sidewalk along Ireland St. Note the speedway gas station to the left of photo.



23. Facing west from the SR 46 and N. Ireland St. intersection looking at eastern end of project area (Des No. 1800255).



24. Facing west from the SR 46 and N. Ireland St. intersection at the surrounding landscape.



25. Facing east from SR 46 and N. Ireland St. intersection looking at the surrounding landscape at eastern end of project area (Des No. 1800255).



26. Facing west looking towards the intersection of East St./S. Michigan Ave. and SR 46 at the sidewalk located along the westbound lane of SR 46. This begins Des No. 1800256 project area.



27. Facing east looking down the sidewalk at the surrounding landscape along the eastbound lane of SR 46. Note Sinclair gas station is directly to the right of the photo.



28. Facing south from Bridge No. 046-16-10454 looking at UNT 1 to Sandy Creek (locally known as Gas Creek).



29. Facing north from Bridge No. 046-16-10454 looking at UNT 1 to Sandy Creek (locally known as Gas Creek).



30. Facing east along the westbound lane of SR 46 at the sidewalk and surrounding landscape.



31. Facing east along the eastbound lane of SR 46 at the sidewalk and surrounding landscape.



32. Facing east along the eastbound lane of SR 46 at the sidewalk and surrounding landscape.



33. Facing east along the westbound lane of SR 46 at the sidewalk and surrounding landscape.



34. Facing west along the eastbound lane of SR 46 at the surrounding landscape.



35. Facing west looking at the surrounding landscape along the westbound lane of SR 46. Note the Greensburg Public Library to the right.



36. Facing west along the eastbound lane of SR 46 looking at the surrounding landscape.



37. Facing east from the SR 46 and Base Road intersection looking at the surrounding landscape and the Decatur County Highway Department office.



38. Facing southeast from the eastbound lane of SR 46 looking at the surrounding landscape and the end of the project area (Des No. 1800256).

PROJECT	DESIGNATION
1800256	1800255
CONTRACT	BRIDGE FILE
R-41463	.

# INDIANA DEPARTMENT OF TRANSPORTATION



## ROAD PLANS

ROUTE: SR 46 FROM: RP 117+00 TO: RP 118+70  
 PROJECT NO. 1800256 P.E.  
 1800256 R/W  
 1800256 CONST.

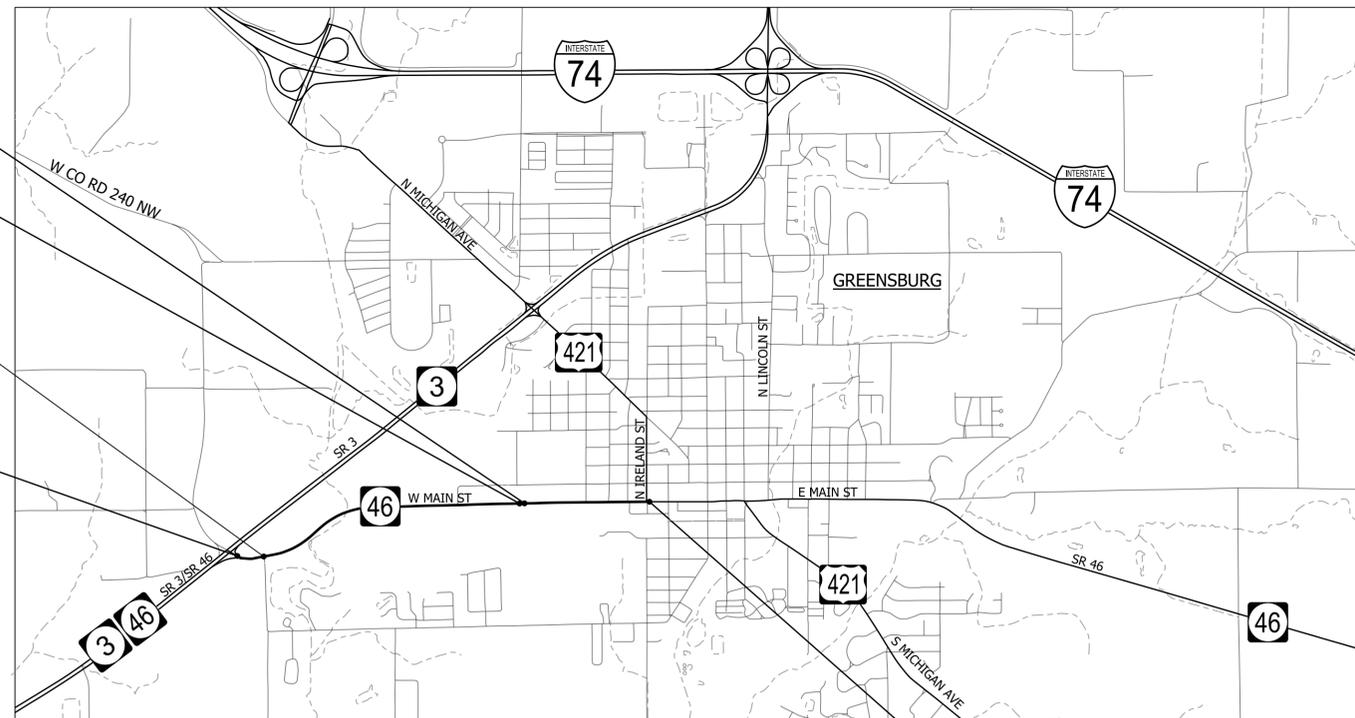
PREVENTATIVE MAINTENANCE & PAVEMENT RECONSTRUCTION ON SR 46 STARTING FROM THE EAST JUNCTION OF SR 3 AND SR 46 TO THE WEST JUNCTION OF SR 46 AND US 421 IN SECTIONS 2, 3, 9, 10, AND 11, T-10N, R-9E, WASHINGTON TOWNSHIP, DECATUR COUNTY, INDIANA.

BEGIN PAVEMENT RECONSTRUCTION  
 STA. 96+05.00 "B"

END PREVENTATIVE MAINTENANCE  
 STA. 95+60.00 "B"

P.T. 55+77.95 "S-SR46-2-C"  
 = O.P.O.T. 34+14.90 "B", 2.50' LT.

BEGIN DES. NO. 1800255  
 BEGIN PREVENTATIVE MAINTENANCE  
 STA. 52+00.00 "S-SR46-2-C"

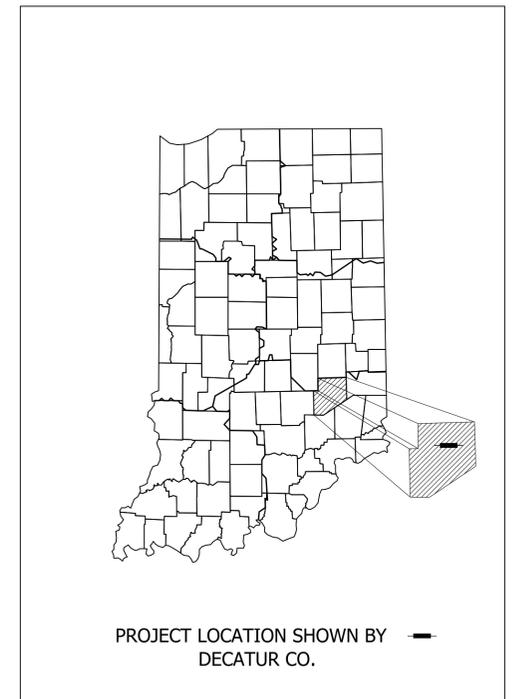


LOCATION MAP

END DES. NO. 1800255  
 END PAVEMENT RECONSTRUCTION  
 STA. 124+50.00 "B"

SCALE: 1" = 2000'

TRAFFIC DATA		STATE ROAD 46	
A.A.D.T. (2023)	5760	V.P.D.	
A.A.D.T. (2043)	7190	V.P.D.	
D.H.V. (2043)	647	V.P.H.	
DIRECTIONAL DISTRIBUTION		50 %	
TRUCKS		5.5 % A.A.D.T. 36 % D.H.V.	
DESIGN DATA		STA. 52+00 "S-SR46-2-C" to STA. 95+80 "B"	
DESIGN SPEED	45	M.P.H.	
PROJECT DESIGN CRITERIA	PARTIAL 3R (NON FREEWAY)		
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL		
RURAL/URBAN	RURAL		
TERRAIN	LEVEL		
ACCESS CONTROL	NONE		
DESIGN DATA		STA. 96+00 "B" to STA. 121+15 "B"	
DESIGN SPEED	30	M.P.H.	
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON FREEWAY)		
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL		
RURAL/URBAN	URBAN (INTERMEDIATE)		
TERRAIN	LEVEL		
ACCESS CONTROL	NONE		



LATITUDE: 39° 20' 11" N LONGITUDE: 85° 29' 55" W

BRIDGE LENGTH:	N/A	MI.
ROADWAY LENGTH:	1.75	MI.
TOTAL LENGTH:	1.75	MI.
MAX. GRADE:	1.43	%

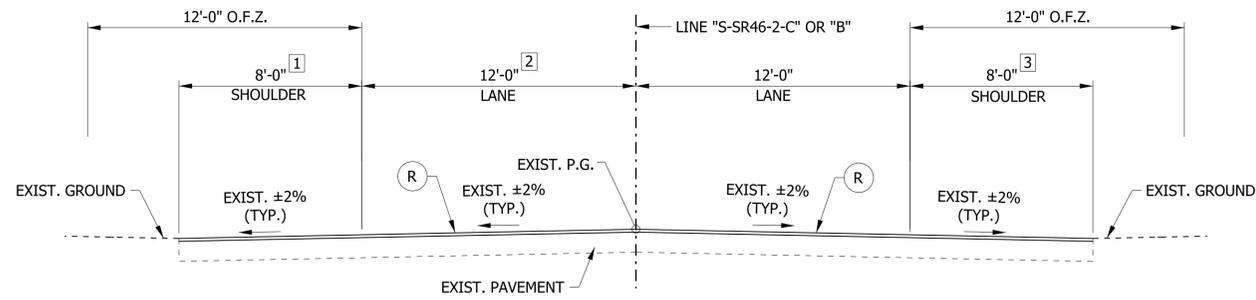
INDIANA DEPARTMENT OF TRANSPORTATION  
 STANDARD SPECIFICATIONS DATED 2022  
 TO BE USED WITH THESE PLANS



Michael Baker International, Inc.  
 3815 River Crossing Parkway, Suite 120  
 Indianapolis, IN 46240  
 Tel: 317-663-8430 Fax: 317-663-8410  
 www.mbakerial.com

PLANS PREPARED BY:	Michael Baker International, Inc.	317-663-8430 PHONE NUMBER
CERTIFIED BY:		DATE
RECOMMENDED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

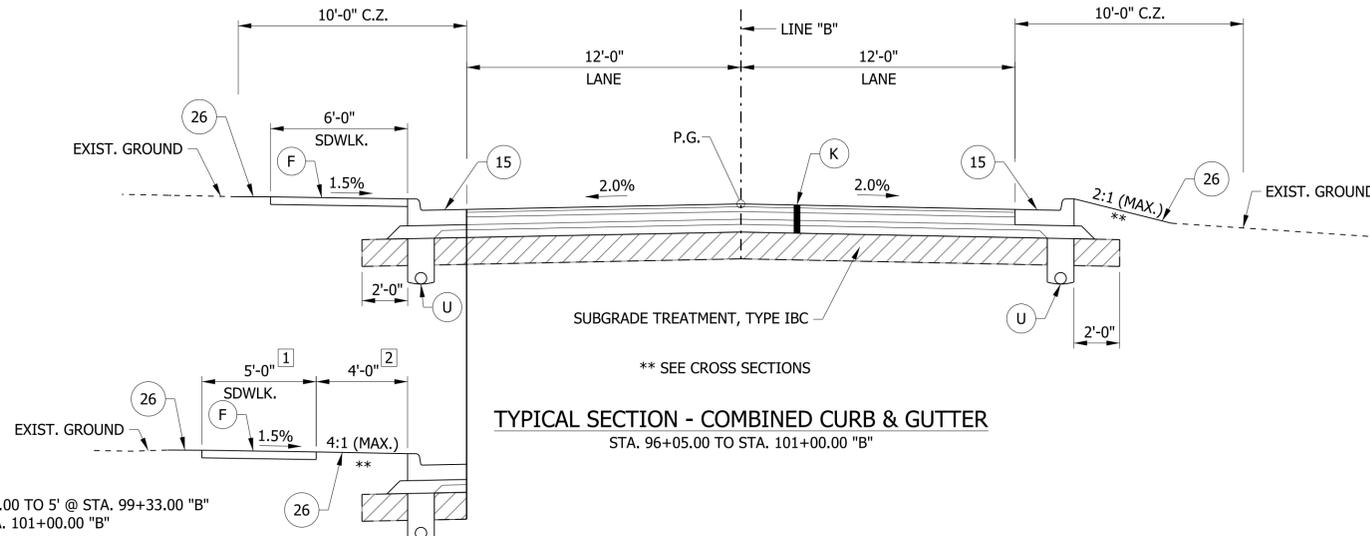
BRIDGE FILE	
DESIGNATION	
1800255	
SHEETS	
1	of 124
PROJECT	
R-41463	
1800256	



- 1 VARIES FROM 7.5' @ STA. 52+00.00 "S-SR46-2-C" TO 9' @ STA. 78+50.00 "B"  
4' FROM STA. 78+50.00 TO STA. 95+60.00 "B"
- 2 VARIES FROM 30' @ STA. 52+00.00 TO 12' @ STA. 55+23.00 "S-SR46-2-C"  
12' FROM STA. 55+23.00 TO STA. 55+77.95 "S-SR46-2-C"  
12' FROM STA. 34+14.90 TO STA. 95+60.00 "B"

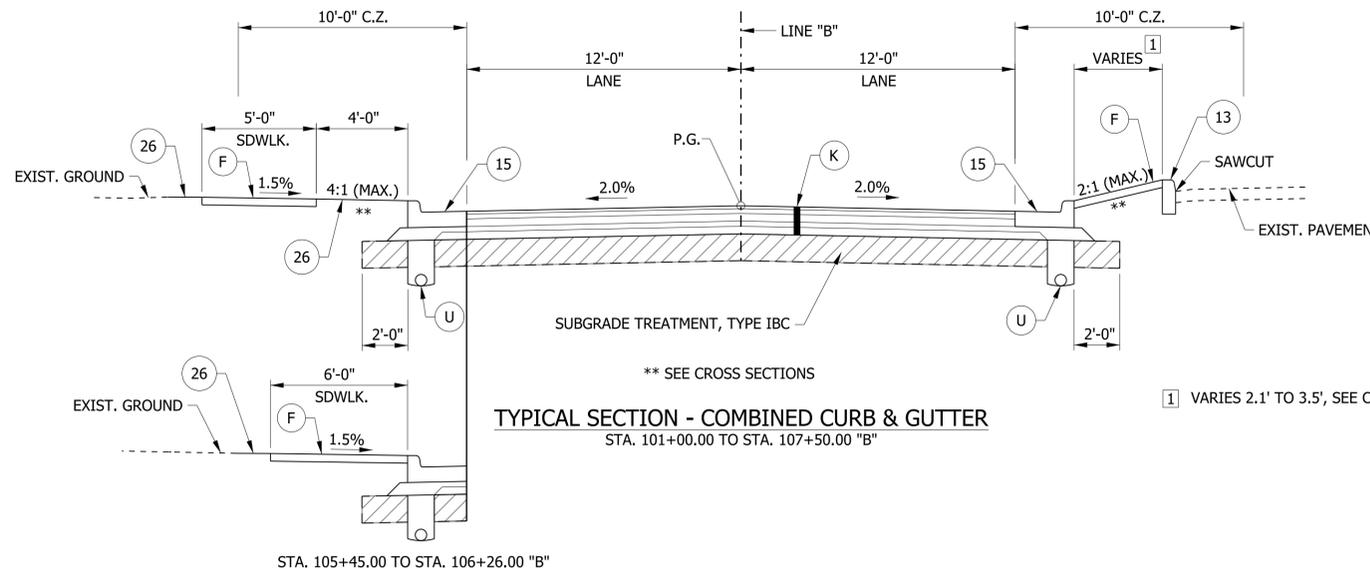
**TYPICAL SECTION - MILL & OVERLAY**  
STA. 52+00.00 TO STA. 55+77.95 "S-SR46-2-C"  
STA. 34+14.90 TO STA. 95+60.00 "B"

- 3 VARIES FROM 8' @ STA. 52+00.00 "S-SR46-2-C" TO 10.5' @ STA. 72+40.00 "B"  
4' FROM STA. 72+40.00 "S-SR46-2-C" TO STA. 80+23.00 "B"  
8' FROM STA. 80+23.00 "B" TO STA. 95+60.00 "B"



- 1 VARIES FROM 6' @ STA. 98+83.00 TO 5' @ STA. 99+33.00 "B"  
5' FROM STA. 99+33.00 TO STA. 101+00.00 "B"
- 2 VARIES FROM 0' @ STA. 98+83.00 TO 4' @ STA. 99+33.00 "B"  
4' FROM STA. 99+33.00 TO STA. 101+00.00 "B"

**TYPICAL SECTION - COMBINED CURB & GUTTER**  
STA. 96+05.00 TO STA. 101+00.00 "B"



- 1 VARIES 2.1' TO 3.5', SEE CONSTR. DETAILS FOR MORE DETAIL

**TYPICAL SECTION - COMBINED CURB & GUTTER**  
STA. 101+00.00 TO STA. 107+50.00 "B"

STA. 105+45.00 TO STA. 106+26.00 "B"

**LEGEND**

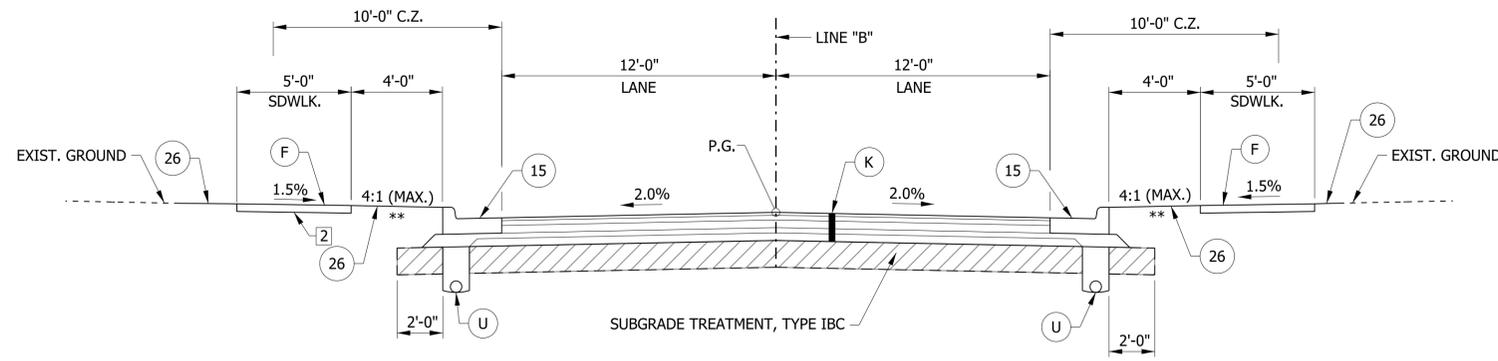
- (K) HMA, FULL DEPTH RECONSTRUCTION:  
165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
275 LB/SYD QC/QA HMA, 3, 70 INTERMEDIATE, 19 MM, ON  
550 LB/SYD QC/QA HMA, 3, 64 BASE, 19 MM, ON  
300 LB/SYD QC/QA HMA, 4, 76, INTERMEDIATE, OG, 19 MM, ON  
4 IN. COMPACTED AGGREGATE, NO. 53 ON,  
SUBGRADE TREATMENT TYPE IBC
- (R) 165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
EXISTING, MILLED ASPHALT SURFACE
- (U) UNDERDRAIN, TYPE 4, 6"
- (F) SIDEWALK, CONCRETE, 4"
- (13) CONCRETE CURB
- (15) MODIFIED COMBINED CONCRETE CURB & GUTTER
- (26) SODDING, NURSERY

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

INDIANA  
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS  
LINE "S-SR46-2-C" & LINE "B"

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE
VERTICAL SCALE 1/4" = 1'-0"	DESIGNATION 1800255
SURVEY BOOK	SHEETS 3 of 124
CONTRACT R-41463	PROJECT 1800256

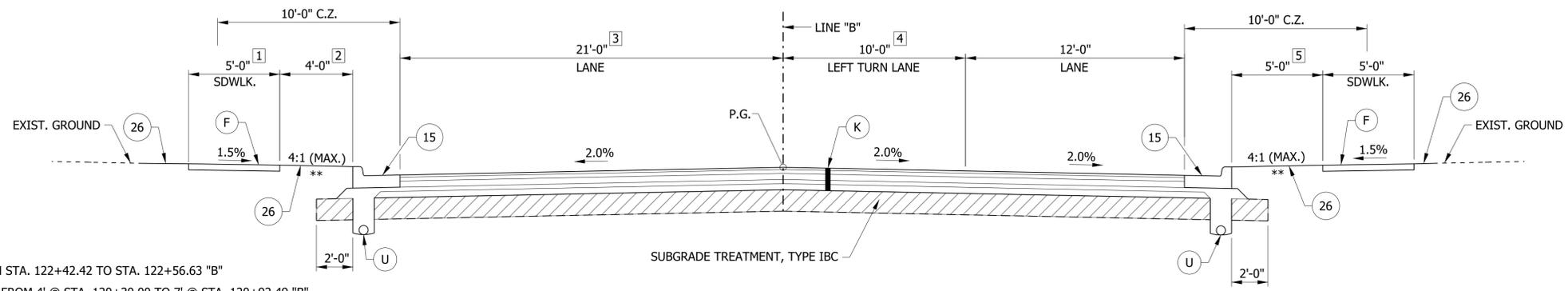


**TYPICAL SECTION - COMBINED CURB & GUTTER**  
STA. 107+50.00 TO STA. 119+35.00 "B"

\*\* SEE CROSS SECTIONS

**LEGEND**

- (K) HMA, FULL DEPTH RECONSTRUCTION:  
165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
275 LB/SYD QC/QA HMA, 3, 70 INTERMEDIATE, 19 MM, ON  
550 LB/SYD QC/QA HMA, 3, 64 BASE, 19 MM, ON  
300 LB/SYD QC/QA HMA, 4, 76, INTERMEDIATE, OG, 19 MM, ON  
4 IN. COMPACTED AGGREGATE, NO. 53 ON,  
SUBGRADE TREATMENT TYPE IBC
- (R) 165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
EXISTING, MILLED ASPHALT SURFACE
- (U) UNDERDRAIN, TYPE 4, 6"
- (F) SIDEWALK, CONCRETE, 4"
- (13) CONCRETE CURB
- (15) MODIFIED COMBINED CONCRETE CURB & GUTTER
- (26) SODDING, NURSERY

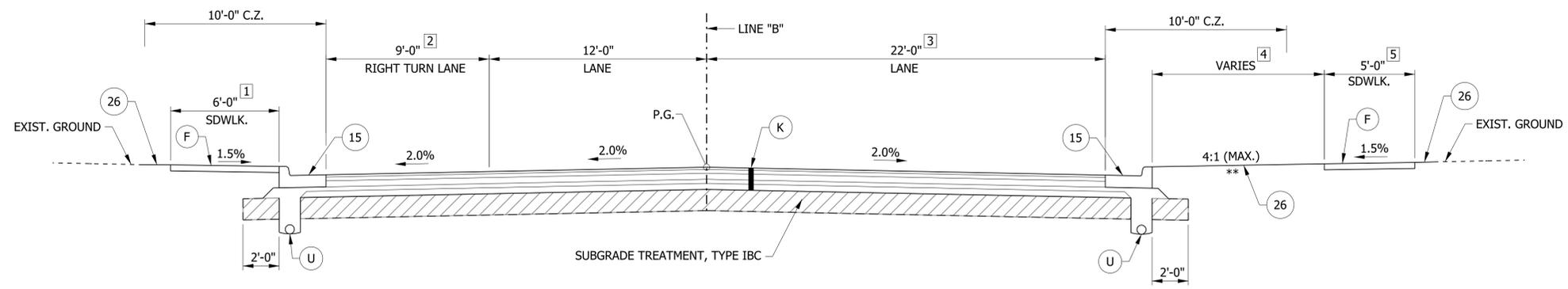


**TYPICAL SECTION - COMBINED CURB & GUTTER**  
STA. 119+35.00 TO STA. 123+00.00 "B"

\*\* SEE CROSS SECTIONS

- 1 6' FROM STA. 122+42.42 TO STA. 122+56.63 "B"
- 2 VARIES FROM 4' @ STA. 120+30.00 TO 7' @ STA. 120+92.49 "B"  
7' FROM STA. 120+92.49 TO STA. 122+00.00 "B"  
VARIES FROM 7' @ STA. 122+00.00 TO 0' @ STA. 122+56.63 "B"
- 3 VARIES FROM 12' @ STA. 119+35.00 TO 21' @ STA. 120+55.00 "B"  
21' FROM STA. 120+55.00 TO STA. 112+00.00 "B"  
VARIES FROM 21' @ STA. 112+00.00 TO 25.29' @ STA. 122+48.18 "B"

- 4 VARIES FROM 0' @ STA. 119+35.00 TO 10' @ STA. 121+15.00 "B"  
10' FROM STA. 121+15.00 TO STA. 123+00.00 "B"
- 5 7' FROM STA. 122+11.33 TO STA. 122+93.12 "B"



**TYPICAL SECTION - COMBINED CURB & GUTTER**  
STA. 123+00.00 TO STA. 124+50.00 "B"

\*\* SEE CROSS SECTIONS

- 1 VARIES FROM 6' @ STA. 124+15.00 TO 6.7' @ STA. 124+20.00 "B"  
VARIES FROM 6.7' @ STA. 124+20.00 TO 6.5' @ STA. 124+51.39 "B"
- 2 VARIES FROM 9' @ STA. 124+15.00 TO 9.49' @ STA. 124+50.00 "B"

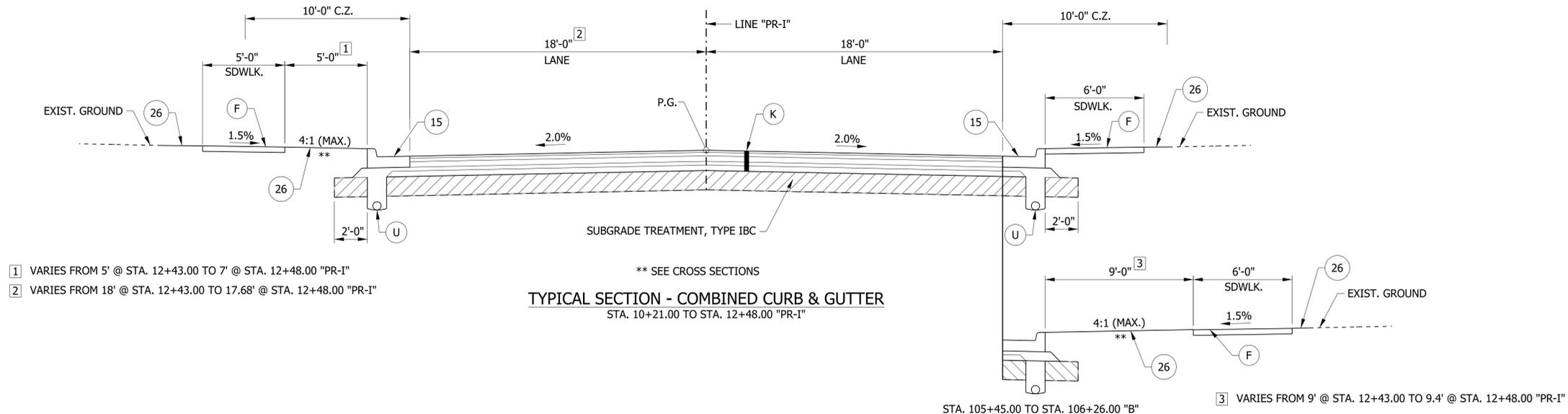
- 3 VARIES FROM 22' @ STA. 124+45.00 TO 22.15' @ STA. 124+50.00 "B"
- 4 VARIES FROM 9.4' @ STA. 123+29.86 TO 9.7' @ STA. 123+98.87 "B"  
VARIES FROM 8.5' @ STA. 124+31.43 TO 7.9' @ STA. 124+41.72 "B"  
VARIES FROM 7.9' @ STA. 124+41.72 TO 8.2' @ STA. 124+51.60 "B"
- 5 5' FROM STA. 123+29.86 TO STA. 123+98.87 "B"  
7.5' FROM STA. 124+31.43 TO STA. 124+41.72 "B"  
VARIES FROM 7.5' @ STA. 124+41.72 TO 7.3' @ STA. 124+51.60 "B"  
(MATCH BACK OF EXISTING SIDEWALK FROM STA. 123+29.86 TO STA. 124+51.60 "B")

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

INDIANA  
DEPARTMENT OF TRANSPORTATION

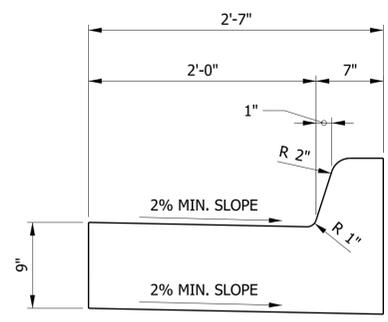
TYPICAL CROSS SECTIONS  
LINE "B"

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	
VERTICAL SCALE	DESIGNATION
1/4" = 1'-0"	1800255
SURVEY BOOK	SHEETS
	4 of 124
CONTRACT	PROJECT
R-41463	1800256

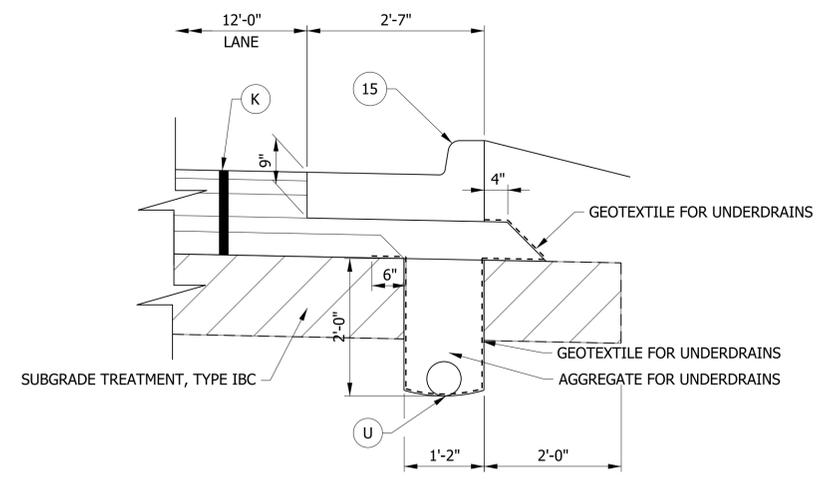


- 1 VARIES FROM 5' @ STA. 12+43.00 TO 7' @ STA. 12+48.00 "PR-1"
- 2 VARIES FROM 18' @ STA. 12+43.00 TO 17.68' @ STA. 12+48.00 "PR-1"

**TYPICAL SECTION - COMBINED CURB & GUTTER**  
 STA. 10+21.00 TO STA. 12+48.00 "PR-1"



**MODIFIED CONCRETE CURB AND GUTTER**  
 SCALE: N.T.S.



**UNDERDRAIN DETAIL W/  
 COMBINED CURB & GUTTER**  
 SCALE: N.T.S.

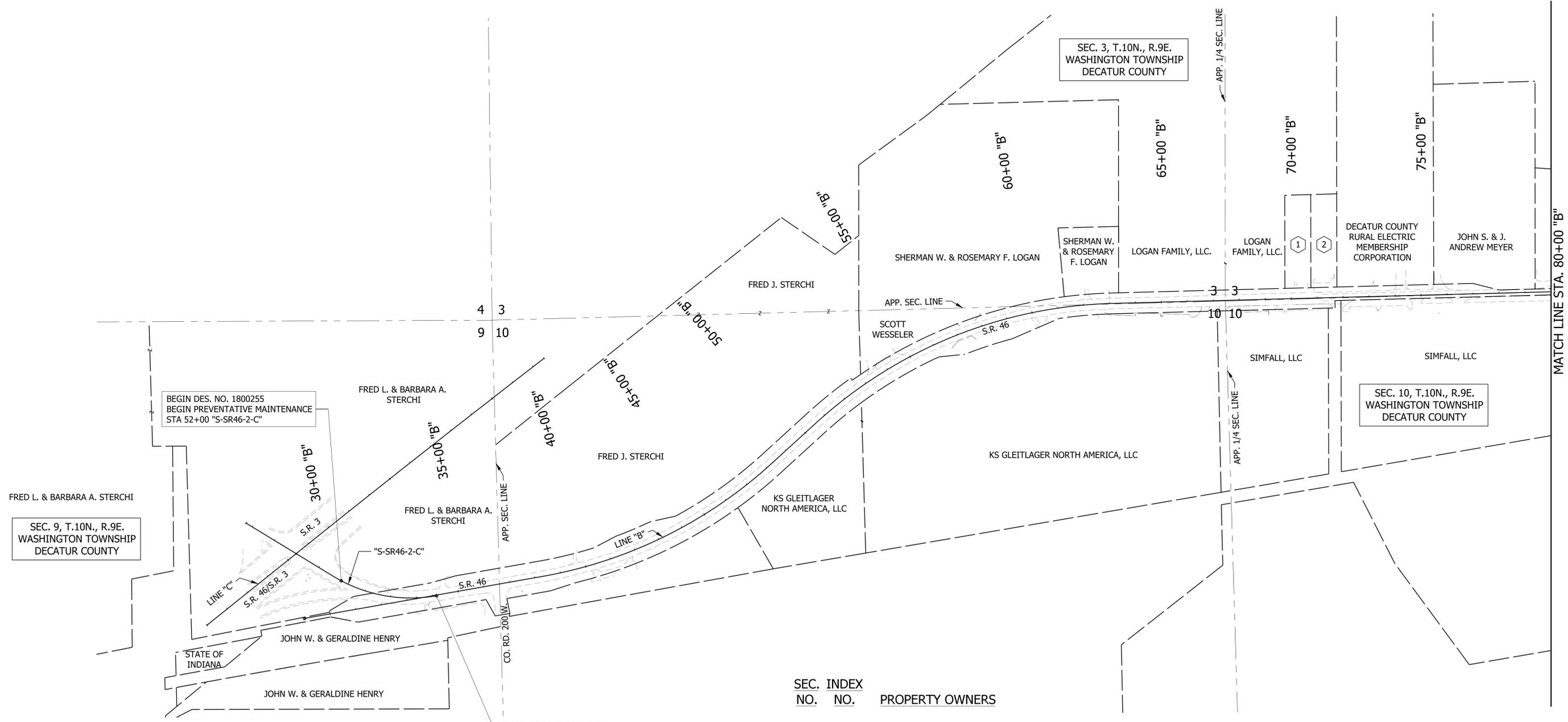
**LEGEND**

- (K) HMA, FULL DEPTH RECONSTRUCTION:  
 165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
 275 LB/SYD QC/QA HMA, 3, 70 INTERMEDIATE, 19 MM, ON  
 550 LB/SYD QC/QA HMA, 3, 64 BASE, 19 MM, ON  
 300 LB/SYD QC/QA HMA, 4, 76, INTERMEDIATE, OG, 19 MM, ON  
 4 IN. COMPACTED AGGREGATE, NO. 53 ON,  
 SUBGRADE TREATMENT TYPE IBC
- (R) 165 LB/SYD QC/QA HMA, 3, 70 SURFACE, 9.5 MM, ON  
 EXISTING, MILLED ASPHALT SURFACE
- (U) UNDERDRAIN, TYPE 4, 6"
- (F) SIDEWALK, CONCRETE, 4"
- (13) CONCRETE CURB
- (15) MODIFIED COMBINED CONCRETE CURB & GUTTER
- (26) SODDING, NURSERY

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "PR-1"	

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE
VERTICAL SCALE 1/4" = 1'-0"	DESIGNATION 1800255
SURVEY BOOK	SHEETS
	5 of 124
CONTRACT R-41463	PROJECT 1800256



BEGIN DES. NO. 1800255  
 BEGIN PREVENTATIVE MAINTENANCE  
 STA 52+00 "S-SR46-2-C"

SEC. 9, T.10N., R.9E.  
 WASHINGTON TOWNSHIP  
 DECATUR COUNTY

SEC. 3, T.10N., R.9E.  
 WASHINGTON TOWNSHIP  
 DECATUR COUNTY

SEC. 10, T.10N., R.9E.  
 WASHINGTON TOWNSHIP  
 DECATUR COUNTY

P.T. 55+77.95 "S-SR46-2-C"  
 = O.P.O.T. 34+14.90 "B"  
 2.50' LT.

SEC. NO.	INDEX NO.	PROPERTY OWNERS
3	1	CHILDREN'S ADVOCACY CENTER OF SOUTHEASTERN INDIANA, INC.
3	2	FARM CREDIT SERVICES OF MID-AMERICA, FLCA

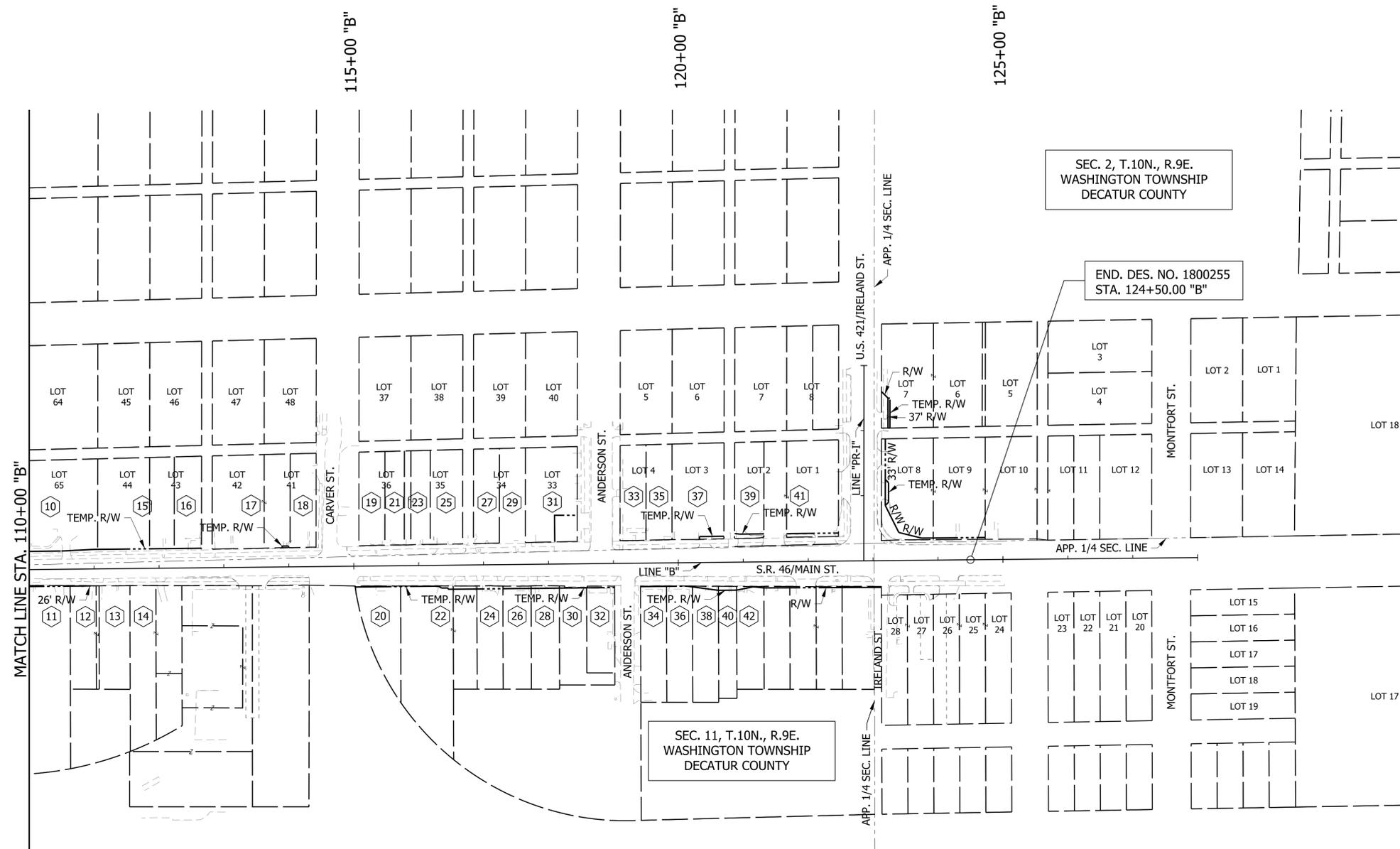
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

INDIANA  
 DEPARTMENT OF TRANSPORTATION

PLAT No. 1

HORIZONTAL SCALE	BRIDGE FILE
1"=200'	
VERTICAL SCALE	DESIGNATION
N/A	1800255
SURVEY BOOK	SHEETS
	6 of 124
CONTRACT	PROJECT
R-41463	1800256





SEC. NO.	INDEX NO.	PROPERTY OWNERS
2	10	TREE COUNTY PLAYERS, INC.
11	11	JENNIFER J. THOMAS
11	12	LOWELL R. & AERETTA L. CLARK
11	13	PAUL DUANE COLLINS
11	14	PHILLIP JACKSON, JR
2	15	PAUL DUANE COLLINS
2	16	DAVID A. COLSON, DONNA S. COLSON, & RUTH G. WINGATE
2	17	ALVIN J. & MARIDEL R. PETERSON
2	18	NENA CAIN
2	19	JOHN W. HOGG, Jr.
11	20	TERRY D. HOGG, JOHN W. HOGG, DEBRA S. HOGG, & SHERRI L. HOGG
2	21	ALVIN J. & MARY R. PETERSON
11	22	SHERRI BRAY
2	23	JACKIE D. RAY
11	24	PHILIP S. NELSON
2	25	TIMOTHY G. SIBBITT
11	26	MATTHEW D. SMALL
2	27	TIMOTHY G. SIBBITT
11	28	HEIST INVESTMENT LLC
2	29	KEVIN R. & KELLEY L. BEDEL
11	30	JOHN E. & SHEILA HUEGEL
2	31	NICHOLAS R. HAMILTON
11	32	CODY & HILARY GOINS
2	33	MYRON L & DINAH L BECKNER
11	34	CHRISTOPHER COMER
2	35	LINDA D. PEIRSON
11	36	JOHN A. MCHUGH
2	37	RT RENTAL, LLC
11	38	MAURICE T. GIDDINGS
2	39	SPEEDWAY SUPERAMERICA
11	40	MYRON BECKNER
2	41	MARATHON FINANCE COMPANY
11	42	BENJAMIN WATKINS

SEC. 2, T.10N., R.9E.  
WASHINGTON TOWNSHIP  
DECATUR COUNTY

END. DES. NO. 1800255  
STA. 124+50.00 "B"

SEC. 11, T.10N., R.9E.  
WASHINGTON TOWNSHIP  
DECATUR COUNTY

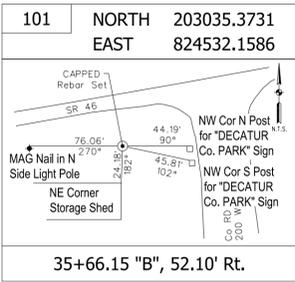
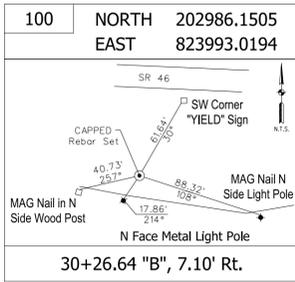
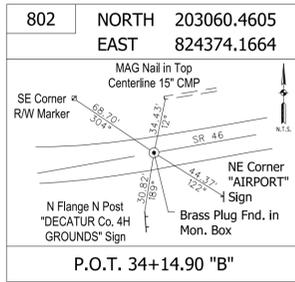
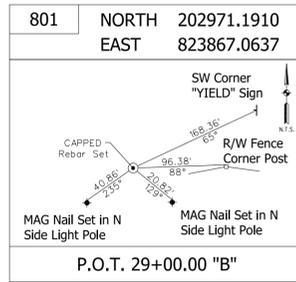
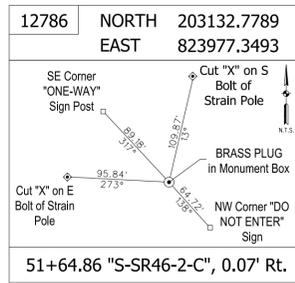
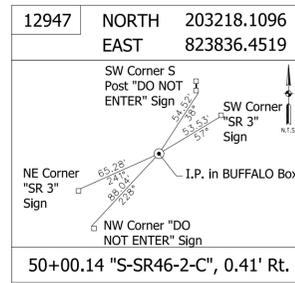
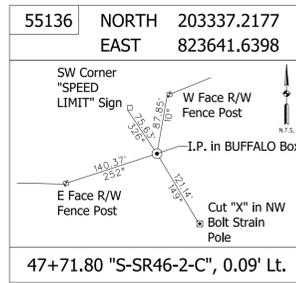


RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

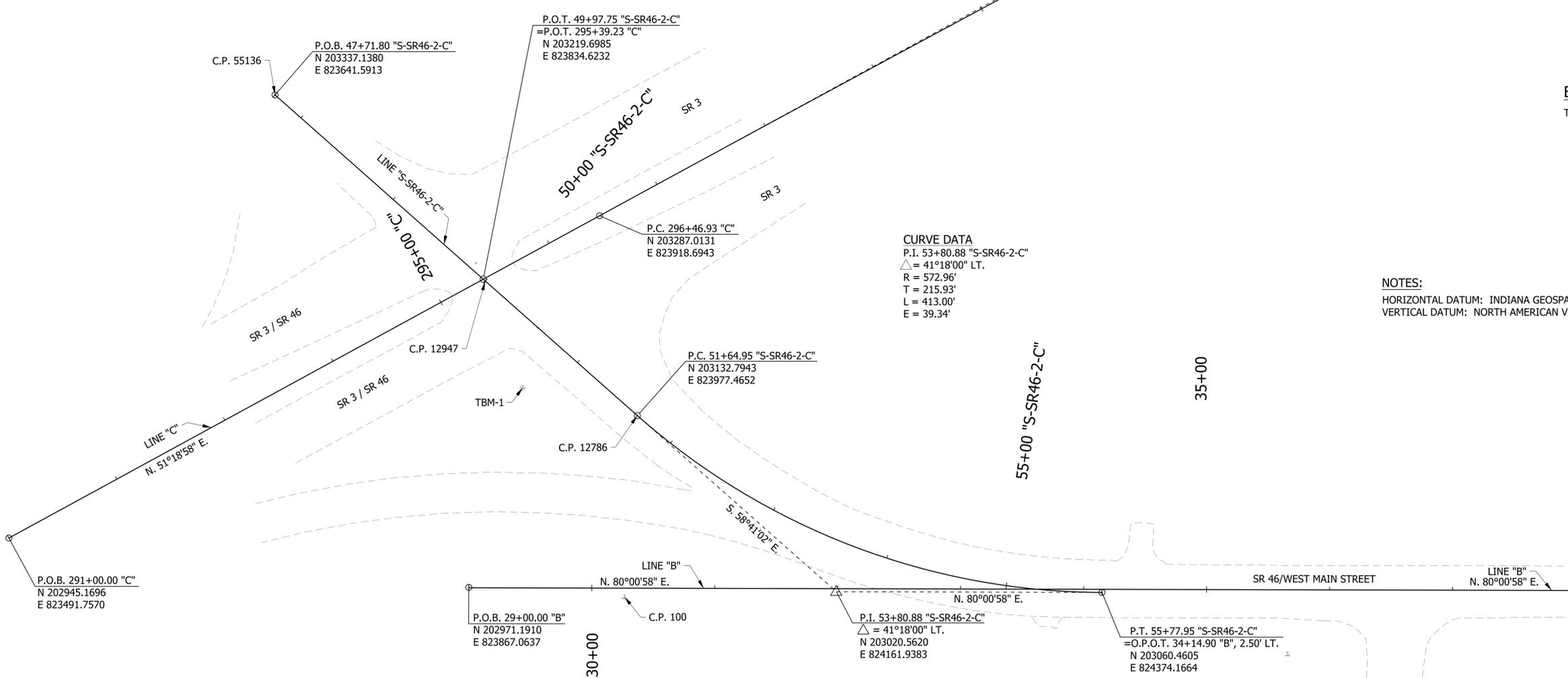
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAT No. 1**

HORIZONTAL SCALE 1"=100'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 1800255
SURVEY BOOK	SHEETS 8 of 124
CONTRACT R-41463	PROJECT 1800256



**CURVE DATA**  
 P.I. 302+00.00 "C"  
 $\Delta = 01^{\circ}06'22''$  RT.  
 R = 57295.78'  
 T = 553.07'  
 L = 1106.11'  
 E = 2.67'



**CURVE DATA**  
 P.I. 53+80.88 "S-SR46-2-C"  
 $\Delta = 41^{\circ}18'00''$  LT.  
 R = 572.96'  
 T = 215.93'  
 L = 413.00'  
 E = 39.34'

**BENCHMARKS**

TBM-1 THE SOUTH BOLT ON THE STRAIN POLE AT THE SOUTHWEST CORNER OF THE INTERSECTION OF S.R. 46 AND S.R. 3. STA. 50+80.08 "S-SR46-2-C", 44.92' RT. ELEV. 917.26

**NOTES:**

HORIZONTAL DATUM: INDIANA GEOSPATIAL COORDINATE SYSTEM (InGCS) DECATUR AND RUSH COUNTIES.  
 VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)

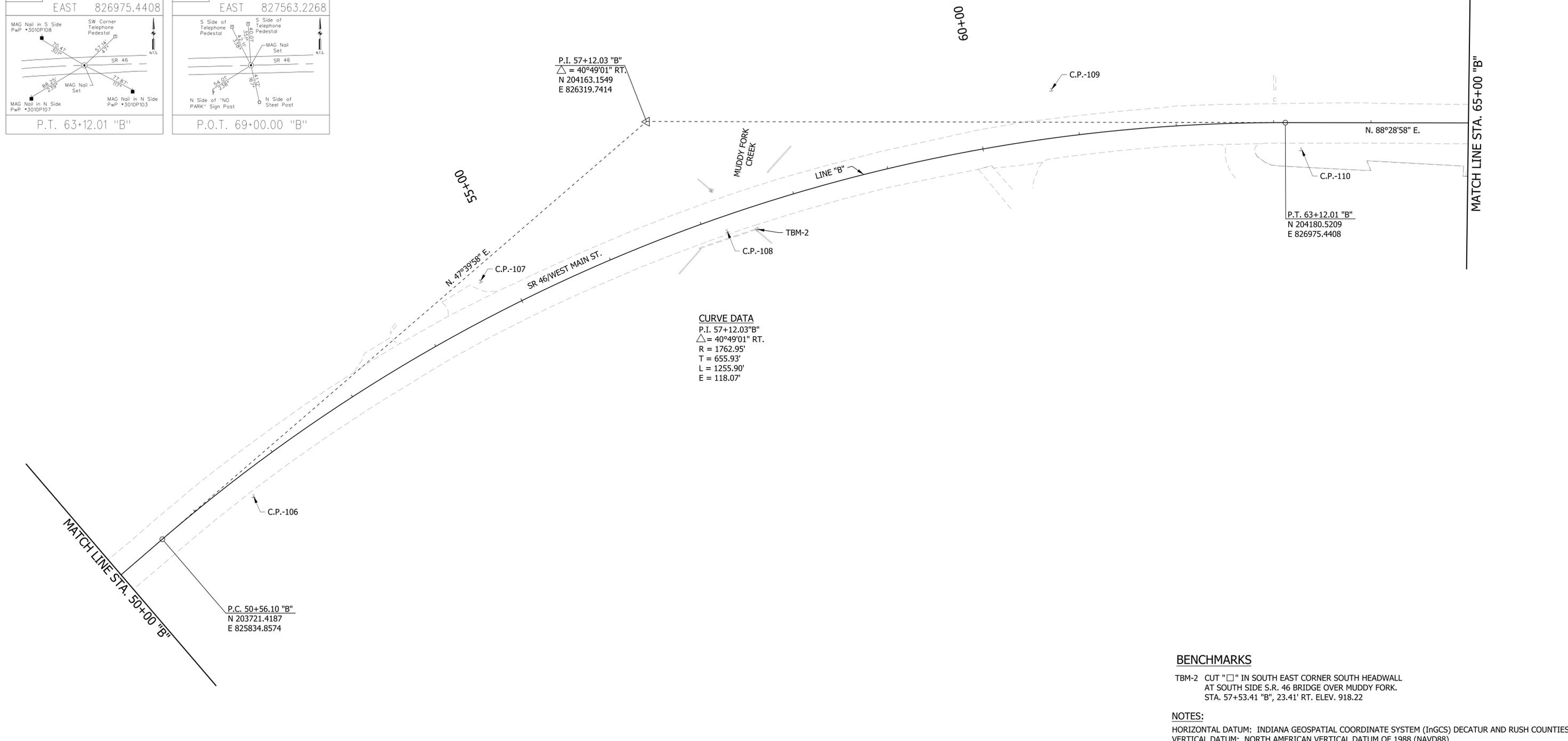
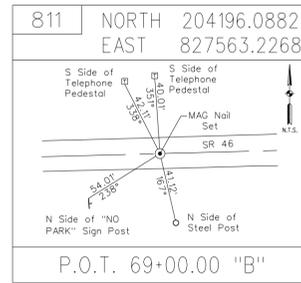
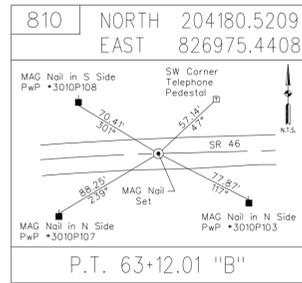
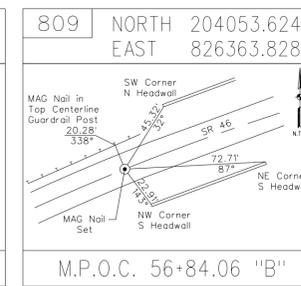
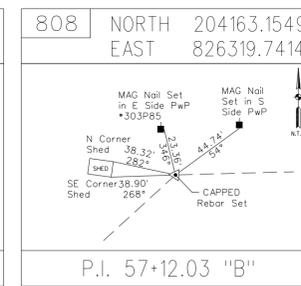
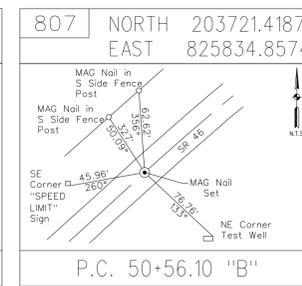
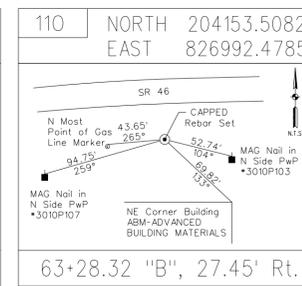
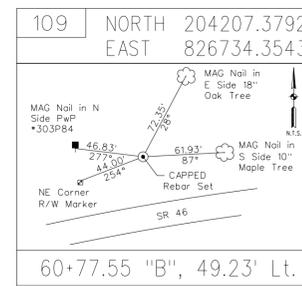
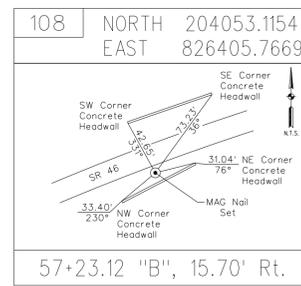
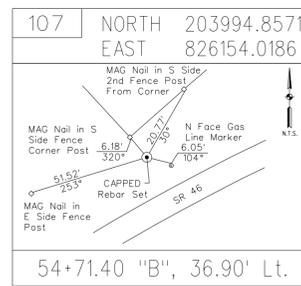
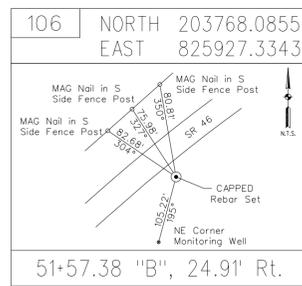
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**SURVEY CONTROL**  
**LINE "C", LINE "S-SR46-2-C" & LINE "B"**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	
VERTICAL SCALE	DESIGNATION
N/A	1800255
SURVEY BOOK	SHEETS
	9 of 124
CONTRACT	PROJECT
R-41463	1800256



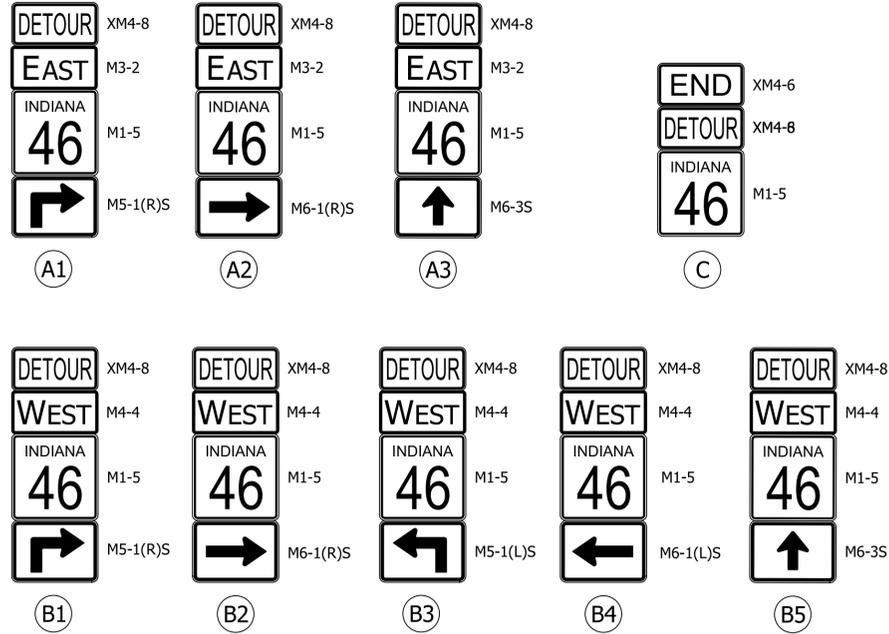


**BENCHMARKS**  
 TBM-2 CUT "□" IN SOUTH EAST CORNER SOUTH HEADWALL  
 AT SOUTH SIDE S.R. 46 BRIDGE OVER MUDDY FORK.  
 STA. 57+53.41 "B", 23.41' RT. ELEV. 918.22

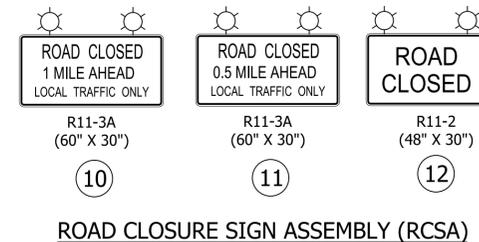
**NOTES:**  
 HORIZONTAL DATUM: INDIANA GEOSPATIAL COORDINATE SYSTEM (InGCS) DECATUR AND RUSH COUNTIES.  
 VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88)

RECOMMENDED FOR APPROVAL _____ DESIGNED: DC CHECKED: WRC	DESIGN ENGINEER _____ DRAWN: JEC CHECKED: WRC	INDIANA DEPARTMENT OF TRANSPORTATION  SURVEY CONTROL LINE "B"	HORIZONTAL SCALE	BRIDGE FILE
			1"=50'	
			VERTICAL SCALE	DESIGNATION
			N/A	1800255
			SURVEY BOOK	SHEETS
				11 of 124
			CONTRACT	PROJECT
			R-41463	1800256

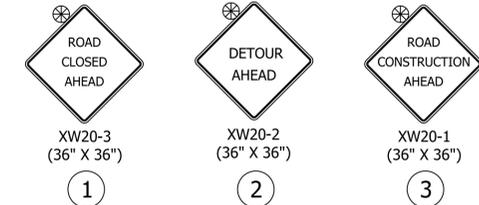




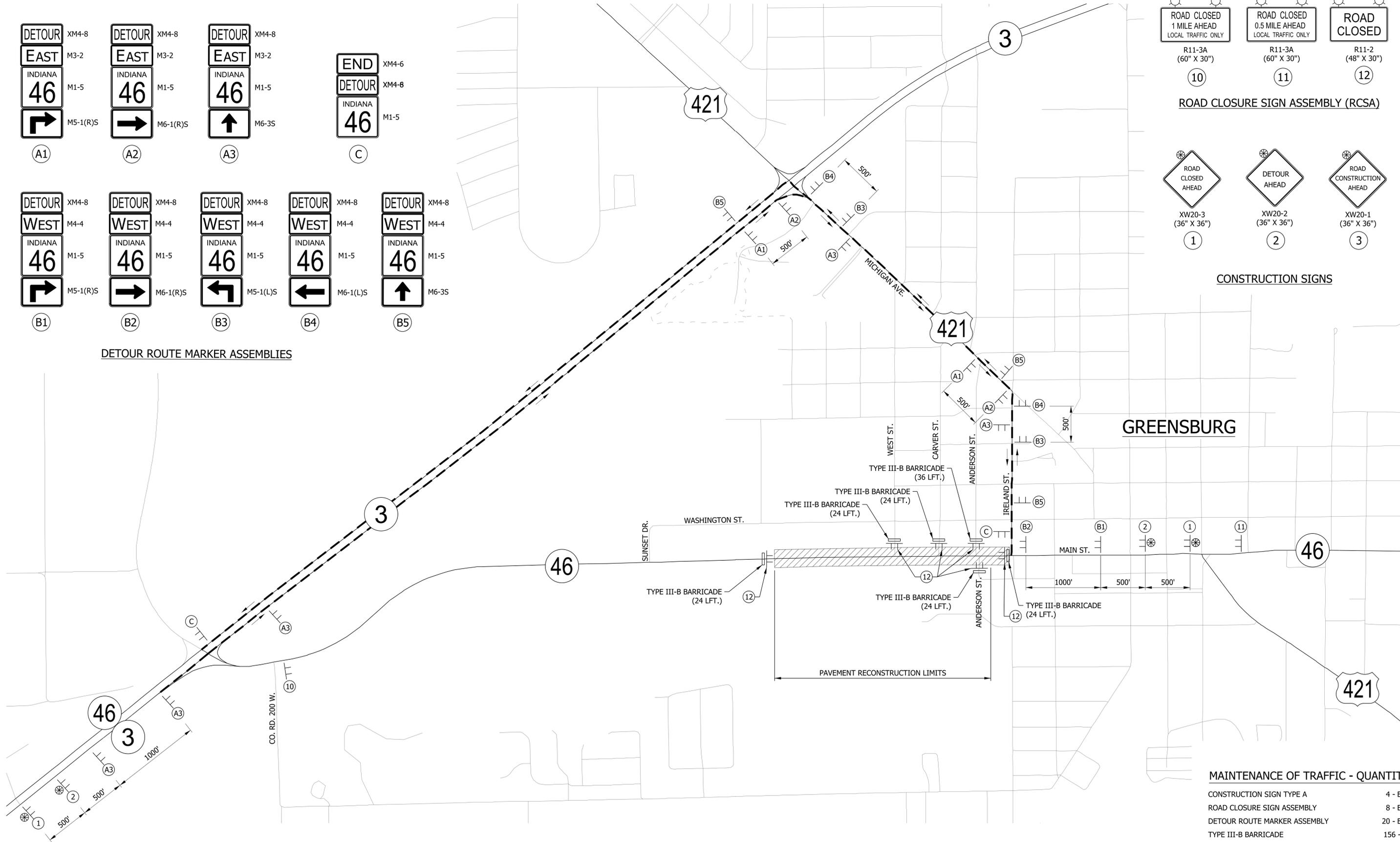
DETOUR ROUTE MARKER ASSEMBLIES



ROAD CLOSURE SIGN ASSEMBLY (RCSA)



CONSTRUCTION SIGNS



MAINTENANCE OF TRAFFIC - QUANTITIES

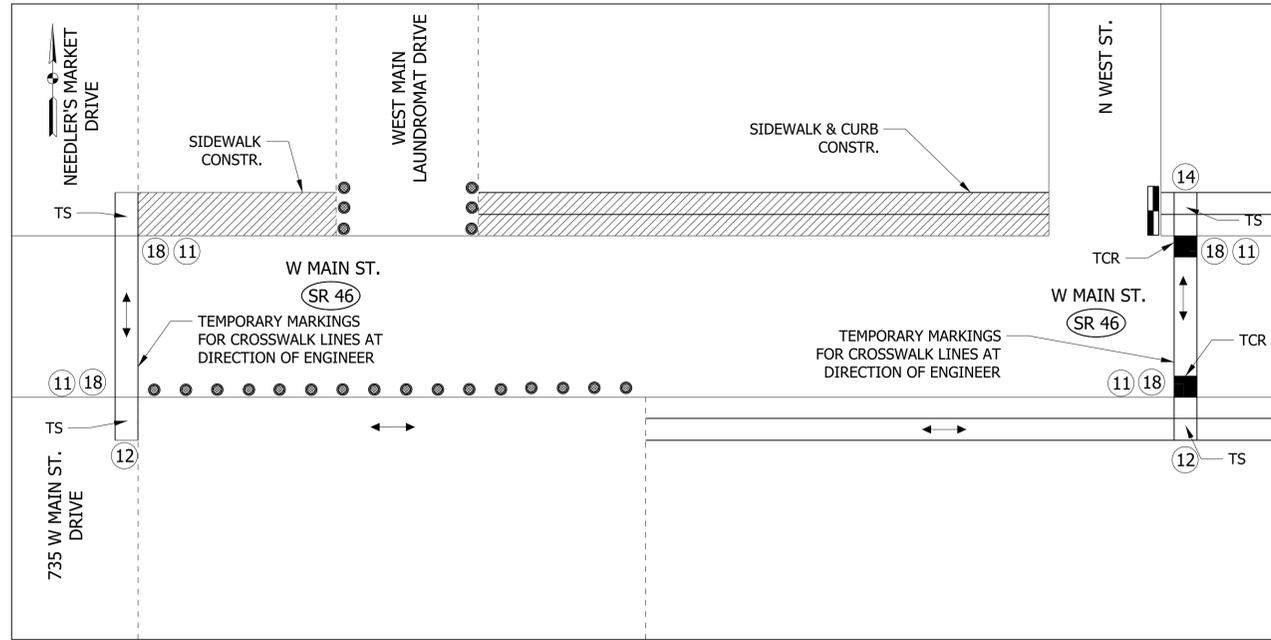
CONSTRUCTION SIGN TYPE A	4 - EACH
ROAD CLOSURE SIGN ASSEMBLY	8 - EACH
DETOUR ROUTE MARKER ASSEMBLY	20 - EACH
TYPE III-B BARRICADE	156 - LFT

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JWM	DRAWN: JWM	
CHECKED: WRC	CHECKED: LDW	

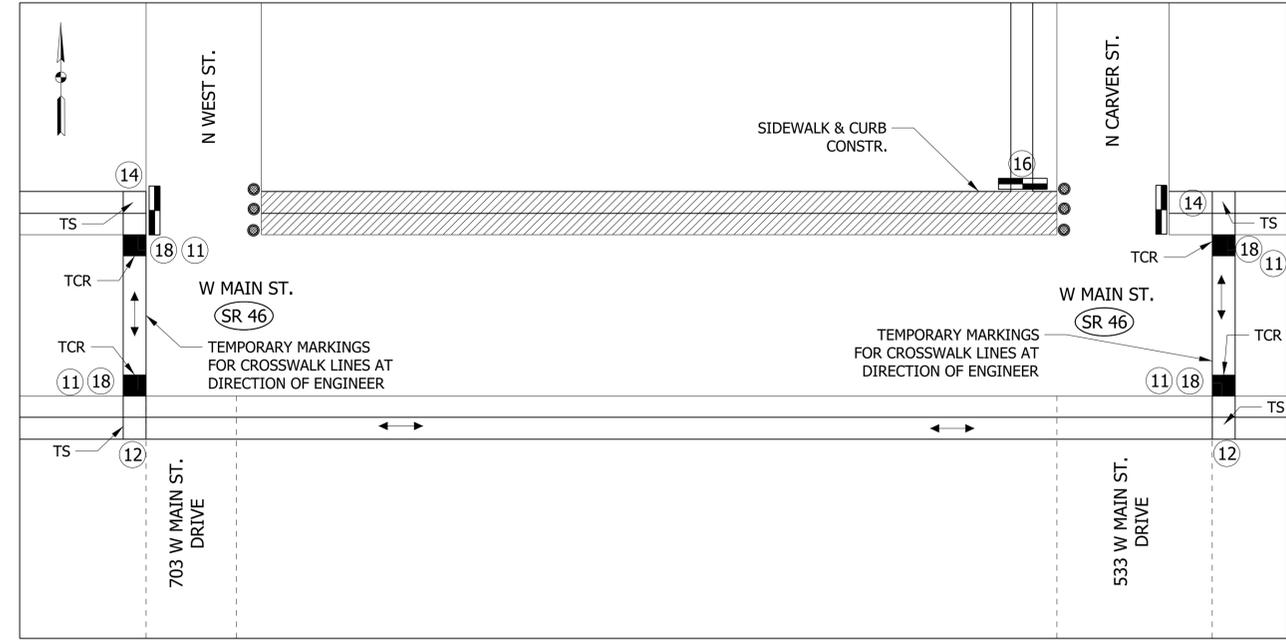
INDIANA  
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC  
DETOUR ROUTE - S.R. 46

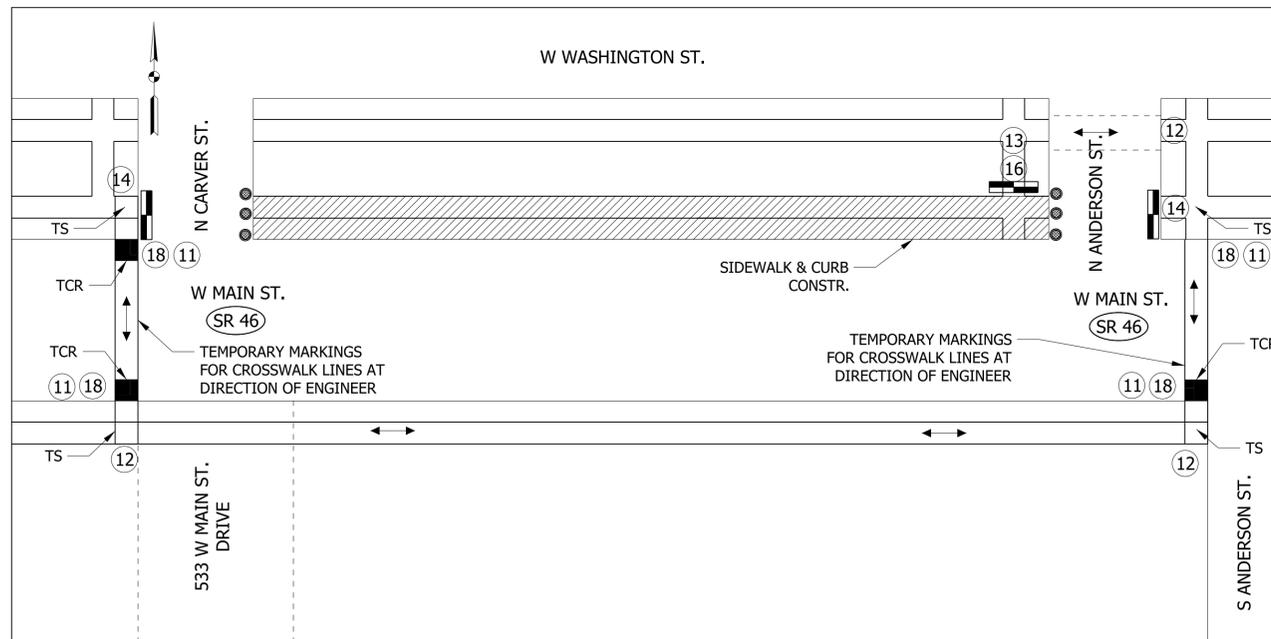
HORIZONTAL SCALE	BRIDGE FILE
1"=500'	
VERTICAL SCALE	DESIGNATION
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SURVEY BOOK	SHEETS
	1 of 3
CONTRACT	PROJECT
R-41463	1800256



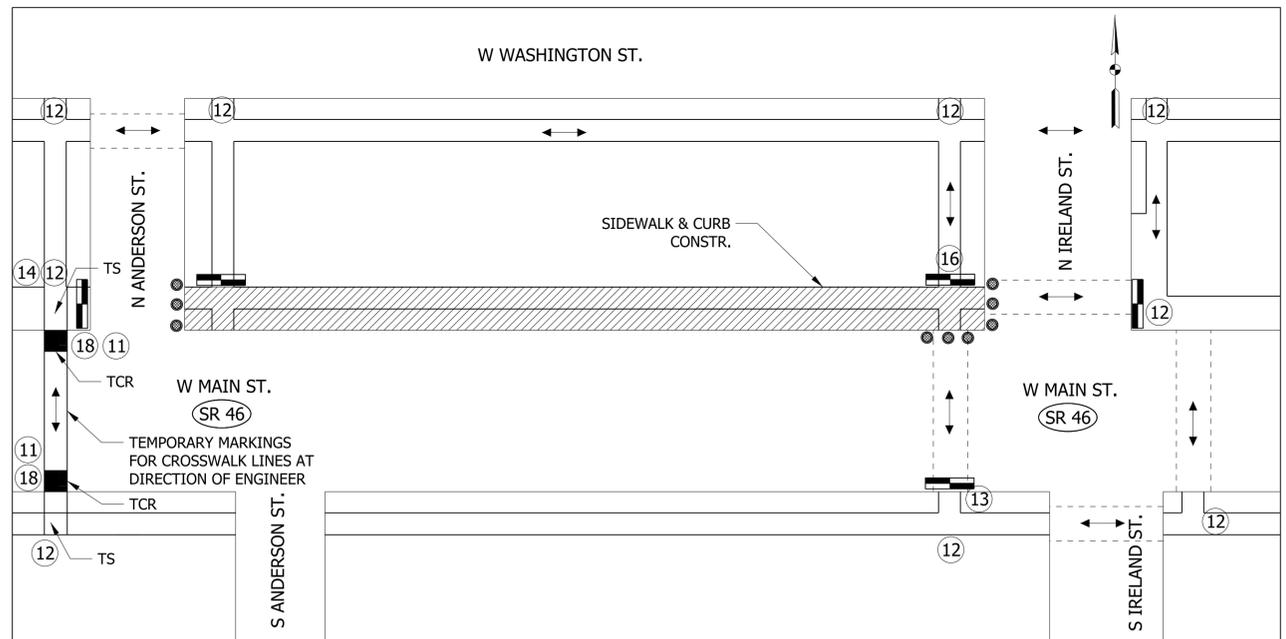
**SIDEWALK CLOSURE**  
 SCALE: NTS  
 LOCATIONS  
 LT SIDEWALK: STA. 105+45 TO N WEST ST.  
 RT SIDEWALK: STA. 107+60 TO STA. 109+58



**SIDEWALK CLOSURE**  
 SCALE: NTS  
 LOCATIONS  
 LT SIDEWALK: N WEST ST. TO N CARVER ST.  
 RT SIDEWALK: STA. 109+58 TO STA. 114+52



**SIDEWALK CLOSURE**  
 SCALE: NTS  
 LOCATIONS  
 LT SIDEWALK: N CARVER ST. TO N ANDERSON ST.



**LT SIDEWALK CLOSURE FROM  
 N ANDERSON ST. TO N IRELAND ST.**  
 SCALE: NTS

**LEGEND**

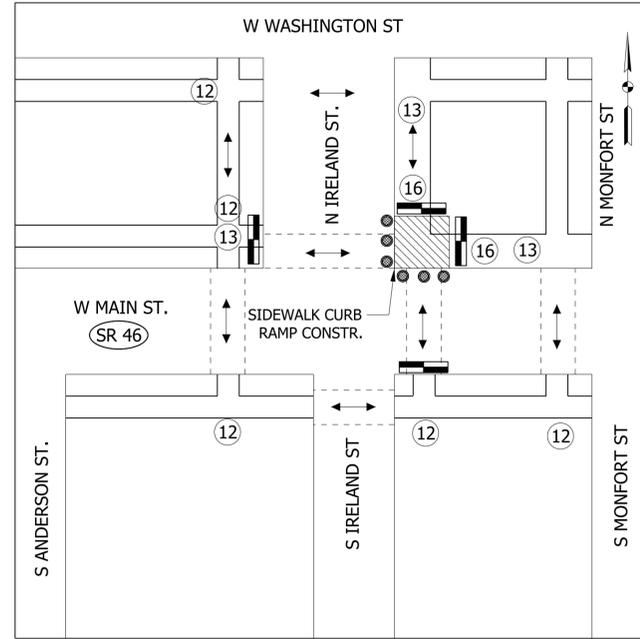
-  TYPE III-A BARRICADE
-  PROPOSED CURB OR SIDEWALK CONSTRUCTION WHERE REQ'D. SEE CONSTRUCTION DETAILS FOR LOCATIONS
- TCR: TEMPORARY CURB RAMP
- TS: TURNING SPACE

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

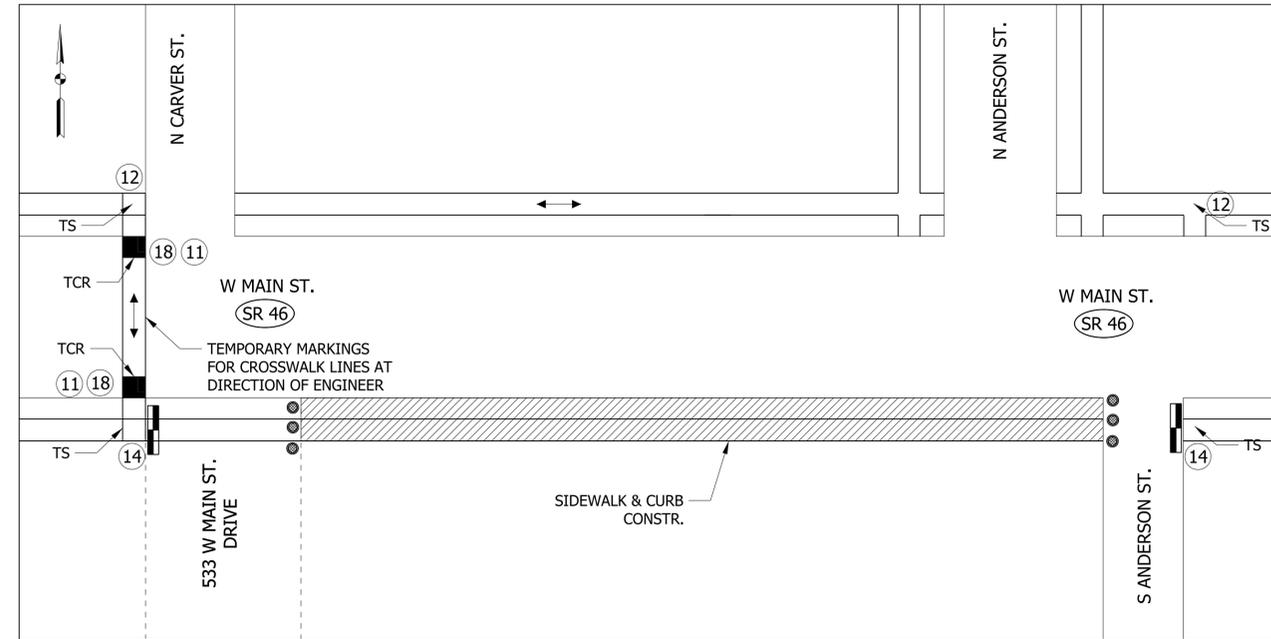
INDIANA  
 DEPARTMENT OF TRANSPORTATION

GENERAL PEDESTRIAN DETAIL

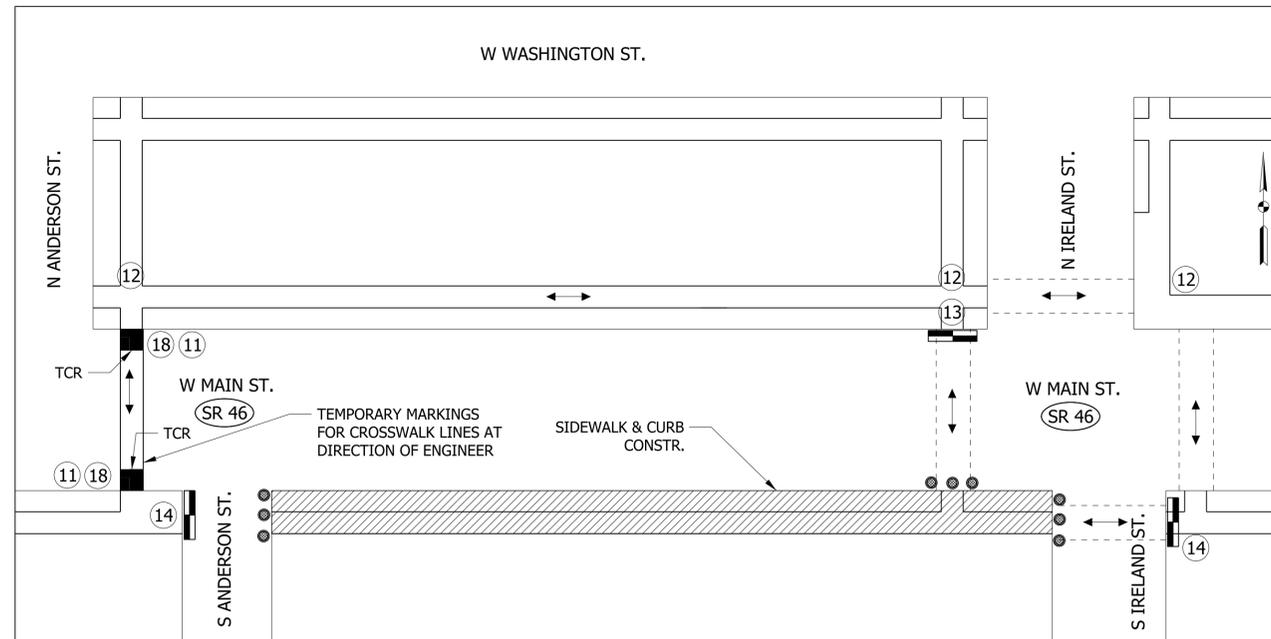
HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	34 of 124
CONTRACT	PROJECT
R-41463	1800256



**SIDEWALK CLOSURE FOR IRELAND ST. & SR 46**  
 SCALE: NTS  
 N IRELAND ST. (NORTHEAST CORNER)  
 S IRELAND ST. (SOUTHEAST CORNER)



**SIDEWALK CLOSURE**  
 SCALE: NTS  
 LOCATIONS  
 RT SIDEWALK: STA. 114+52 TO S ANDERSON ST.



**SIDEWALK CLOSURE**  
 SCALE: NTS  
 LOCATIONS  
 RT SIDEWALK: S ANDERSON ST. TO S IRELAND ST.

**LEGEND**

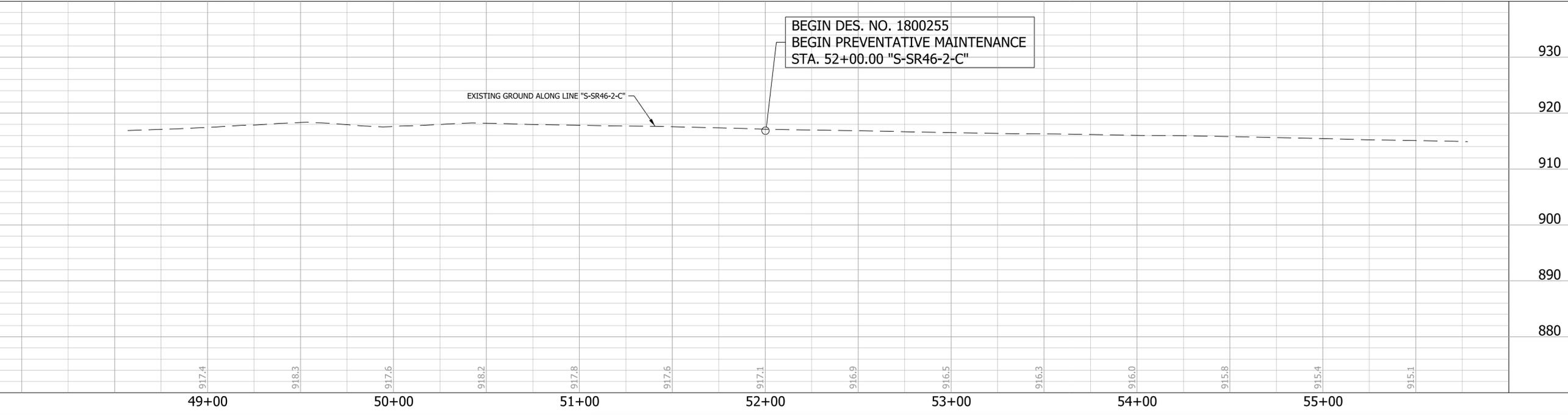
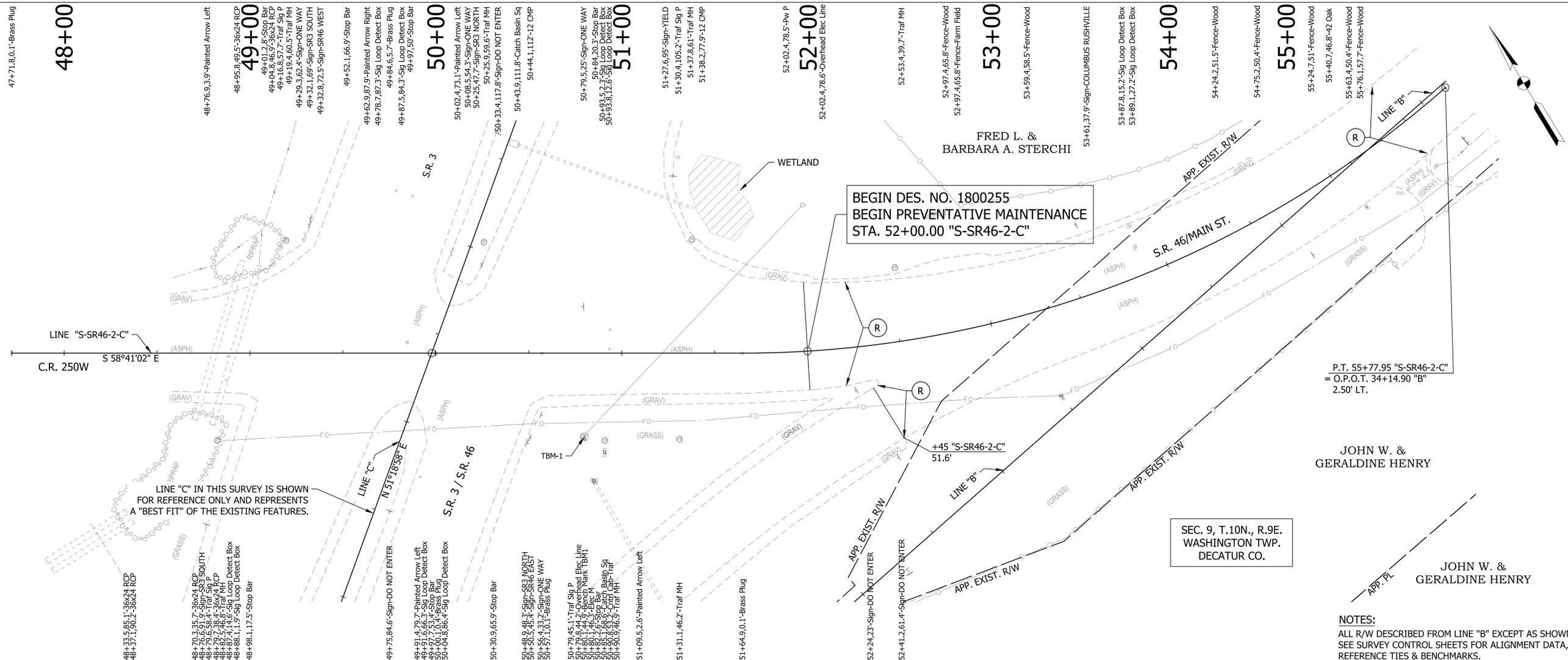
-  TYPE III-A BARRICADE
-  PROPOSED CURB OR SIDEWALK CONSTRUCTION WHERE REQ'D. SEE CONSTRUCTION DETAILS FOR LOCATIONS
- TCR: TEMPORARY CURB RAMP
- TS: TURNING SPACE

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DC	DRAWN: JEC	
CHECKED: WRC	CHECKED: WRC	

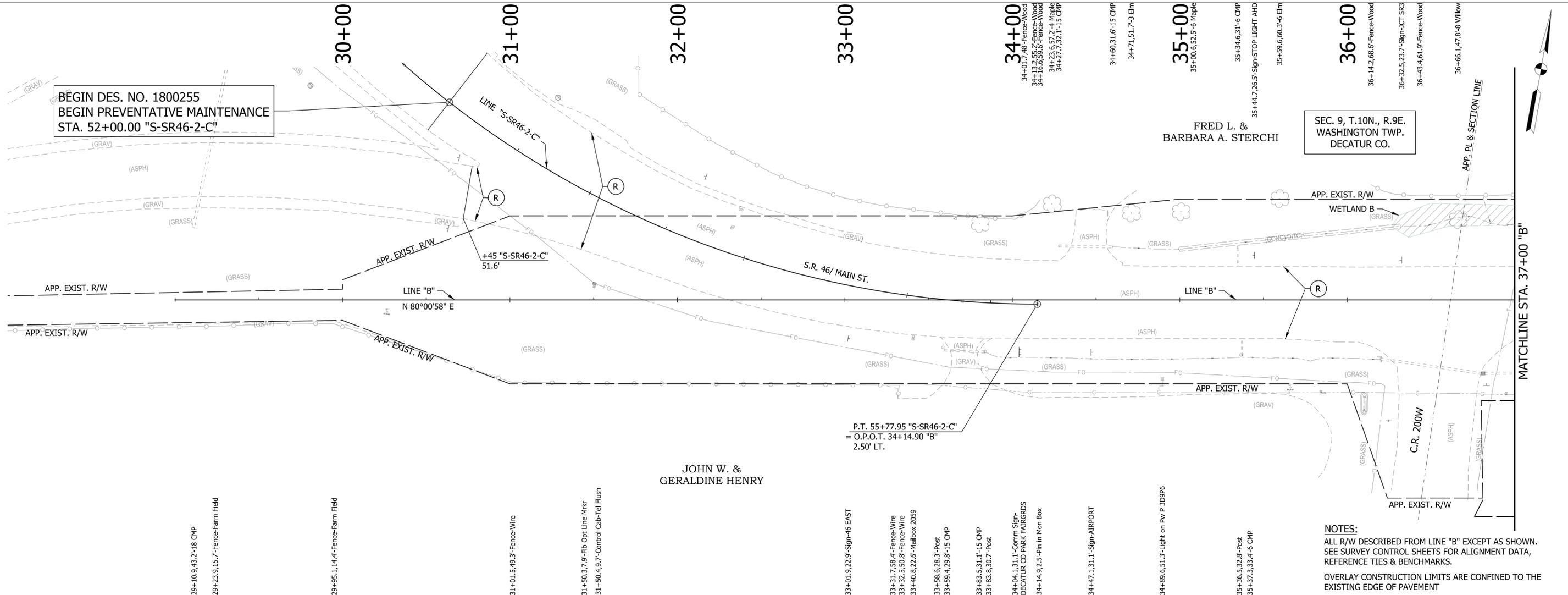
INDIANA  
 DEPARTMENT OF TRANSPORTATION

GENERAL PEDESTRIAN DETAIL

HORIZONTAL SCALE	BRIDGE FILE
N/A	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	35 of 124
CONTRACT	PROJECT
R-41463	1800256



<b>LEGEND</b> (R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON, EXISTING, MILLED ASPHALT SURFACE	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION  <b>PLAN &amp; PROFILE</b> <b>LINE "S-SR46-2-C"</b>	HORIZONTAL SCALE 1"=30' VERTICAL SCALE 1"=10'	BRIDGE FILE DESIGNATION 1800255
	DESIGNED: DC DRAWN: JEC CHECKED: LDW CHECKED: WRC		SURVEY BOOK CONTRACT R-41463	SHEETS 36 of 124 PROJECT 1800256



BEGIN DES. NO. 1800255  
 BEGIN PREVENTATIVE MAINTENANCE  
 STA. 52+00.00 "S-SR46-2-C"

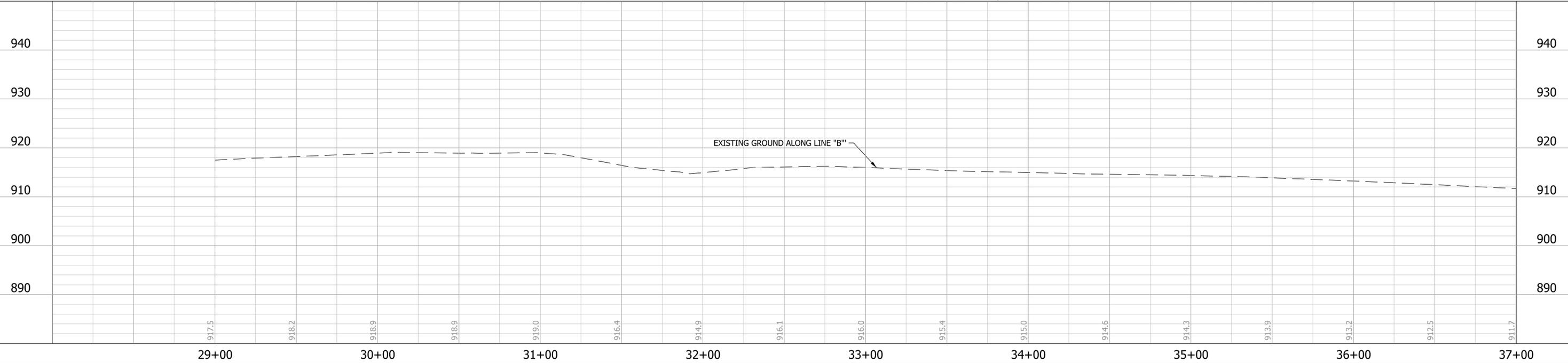
SEC. 9, T.10N., R.9E.  
 WASHINGTON TWP.  
 DECATUR CO.

FRED L. &  
 BARBARA A. STERCHI

JOHN W. &  
 GERALDINE HENRY

P.T. 55+77.95 "S-SR46-2-C"  
 = O.P.O.T. 34+14.90 "B"  
 2.50' LT.

NOTES:  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.  
 OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
 EXISTING EDGE OF PAVEMENT



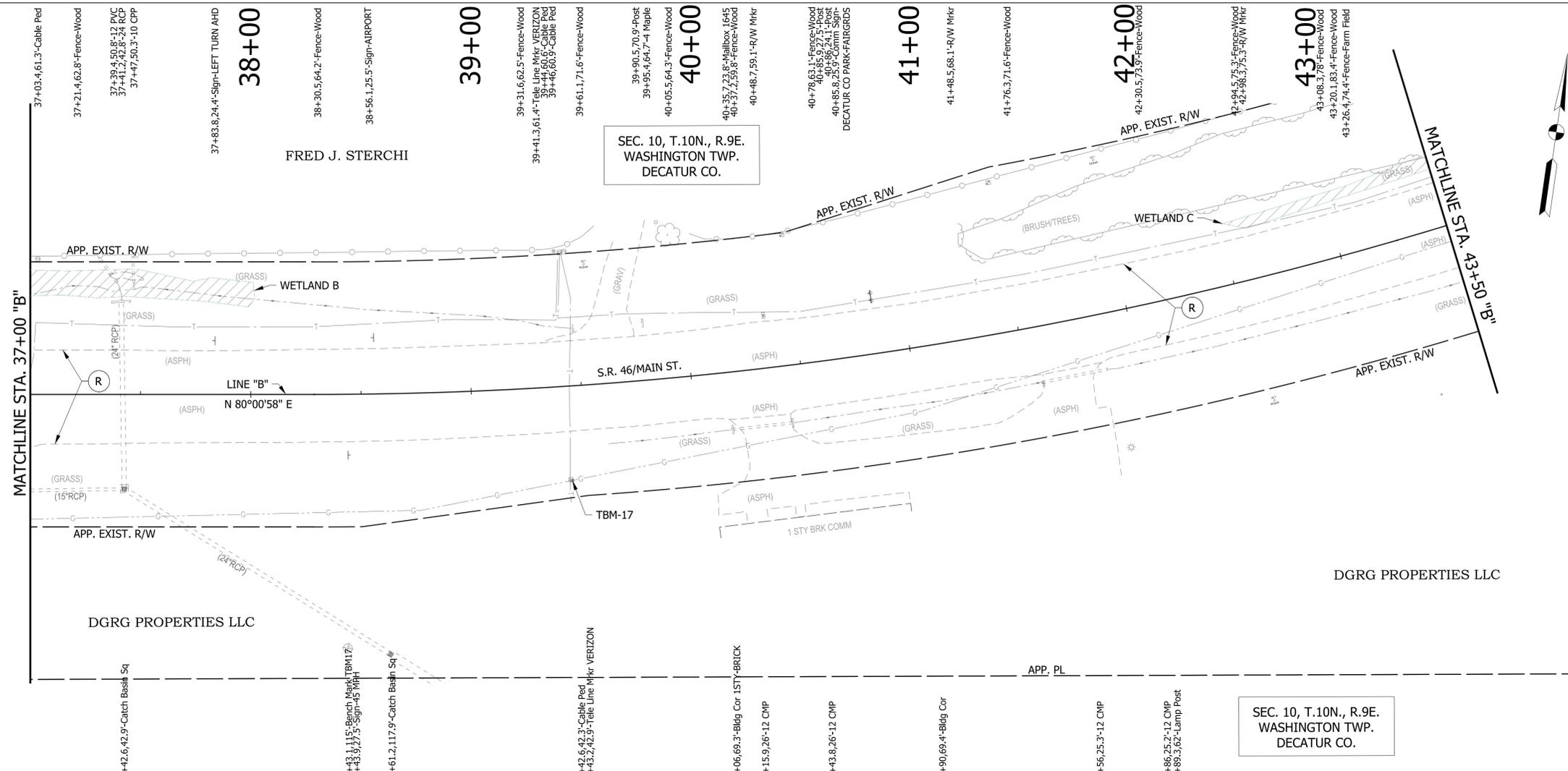
LEGEND  
 (R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON,  
 EXISTING, MILLED ASPHALT SURFACE

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

**INDIANA  
 DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE  
 LINE "B"**

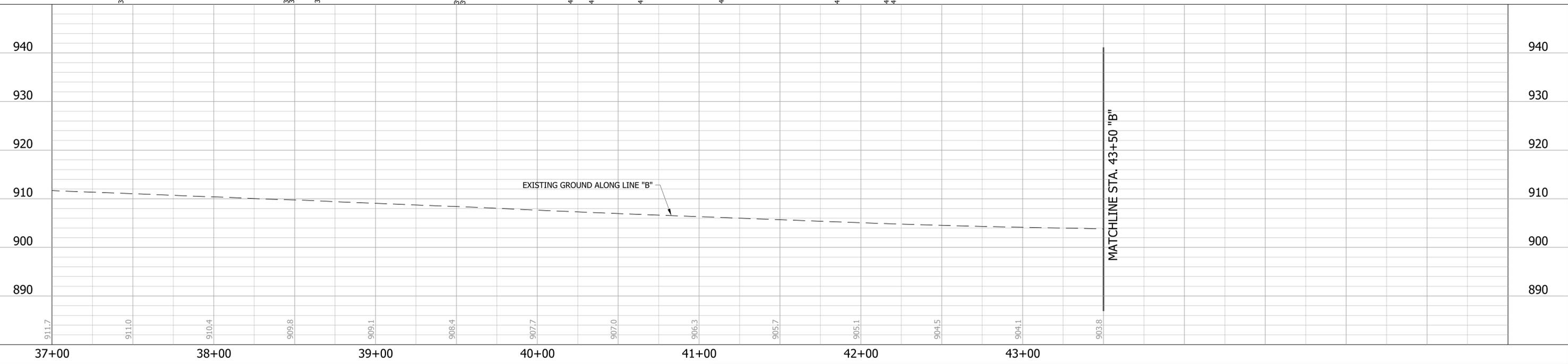
HORIZONTAL SCALE 1"=30'	BRIDGE FILE
VERTICAL SCALE 1"=10'	DESIGNATION 1800255
SURVEY BOOK	SHEETS
CONTRACT R-41463	37 of 124 PROJECT 1800256



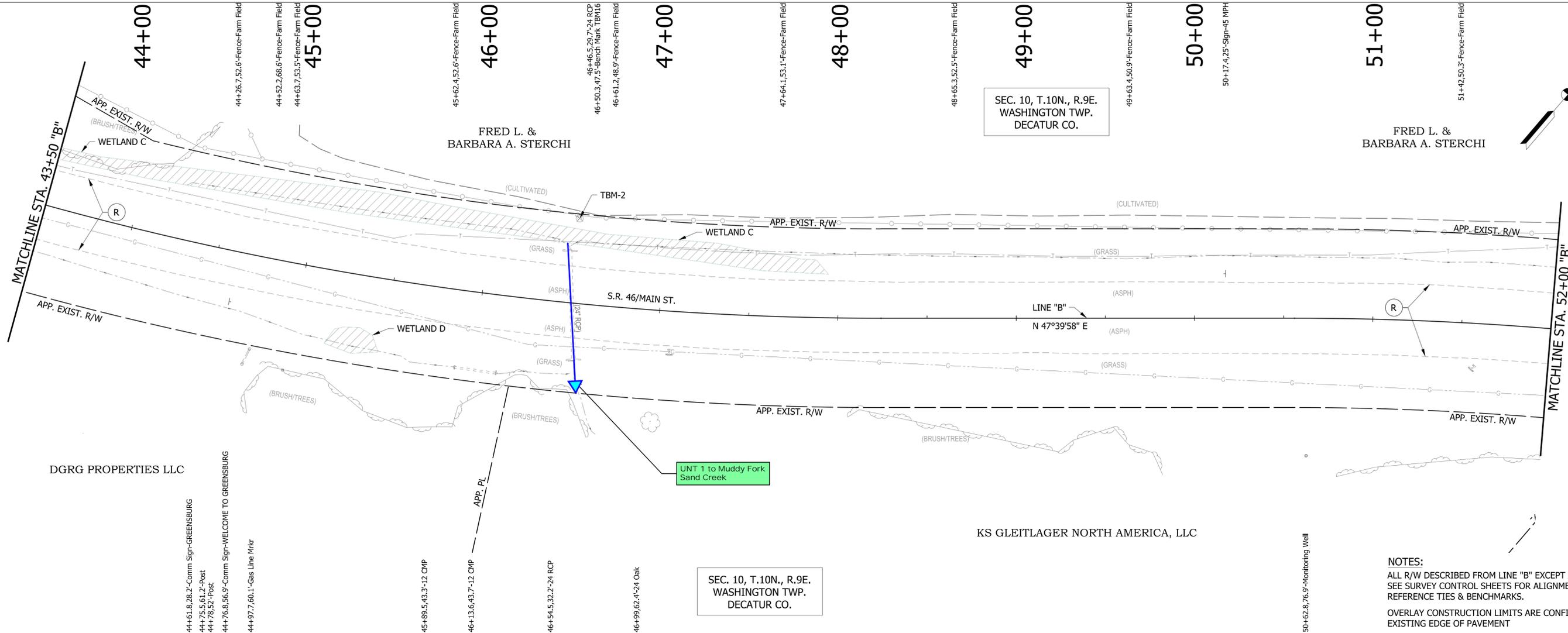
SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

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REFERENCE TIES & BENCHMARKS.  
OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
EXISTING EDGE OF PAVEMENT



RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1"=30'	BRIDGE FILE
	DESIGNED: DC	DRAWN: JEC			DESIGNATION 1800255	
CHECKED: LDW	CHECKED: WRC		PLAN & PROFILE LINE "B"		SURVEY BOOK	SHEETS
					CONTRACT R-41463	38 of 124

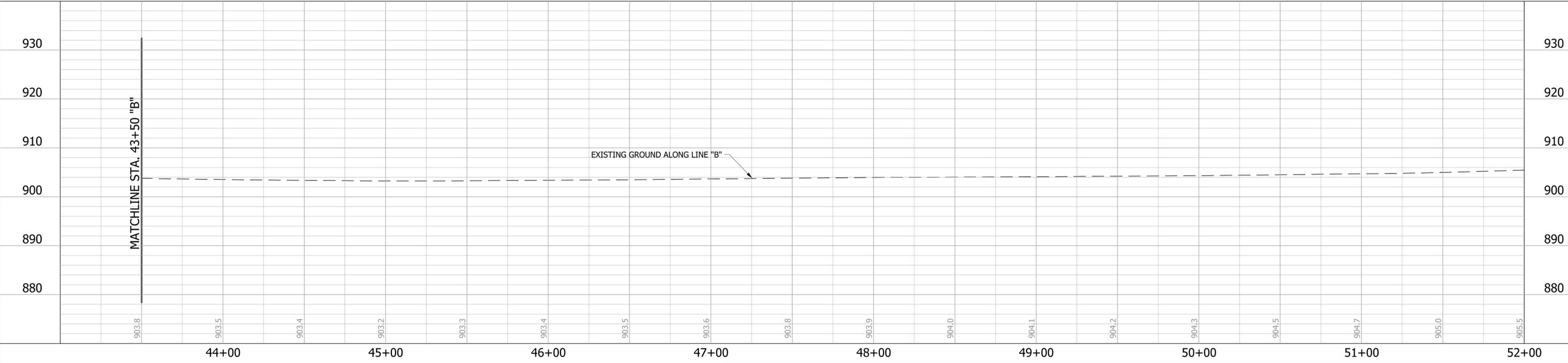


SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

FRED L. &  
BARBARA A. STERCHI

SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

NOTES:  
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SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
REFERENCE TIES & BENCHMARKS.  
OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
EXISTING EDGE OF PAVEMENT



**LEGEND**

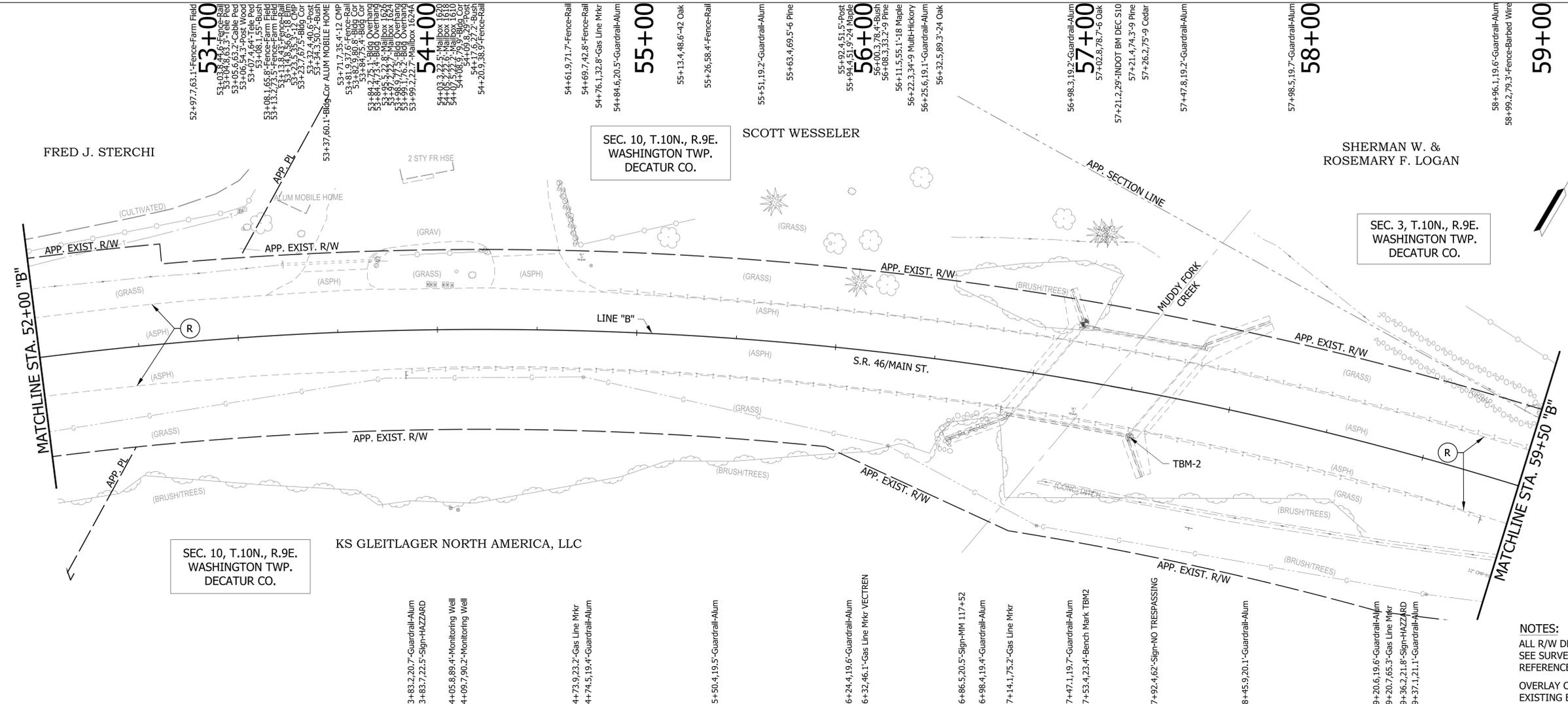
(R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON, EXISTING, MILLED ASPHALT SURFACE

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

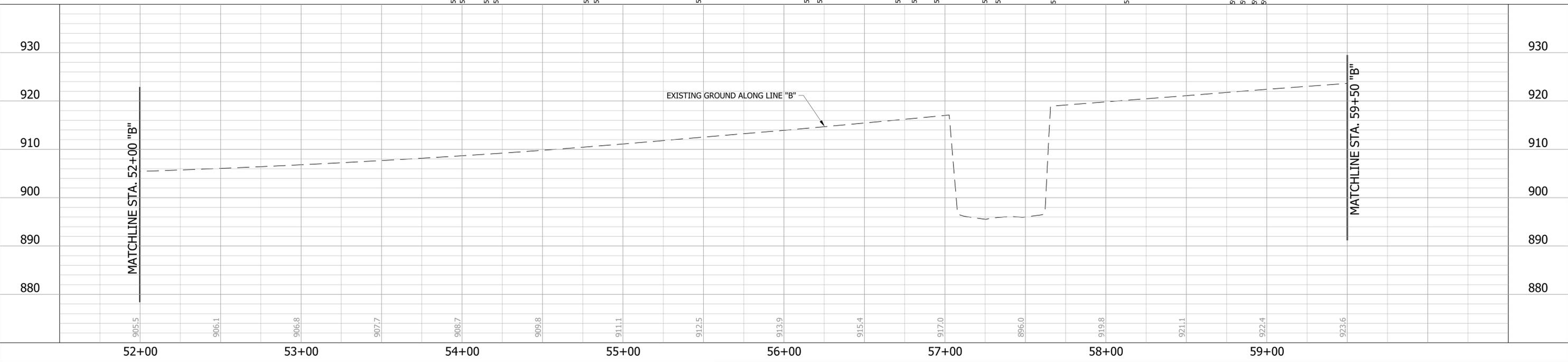
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE  
LINE "B"**

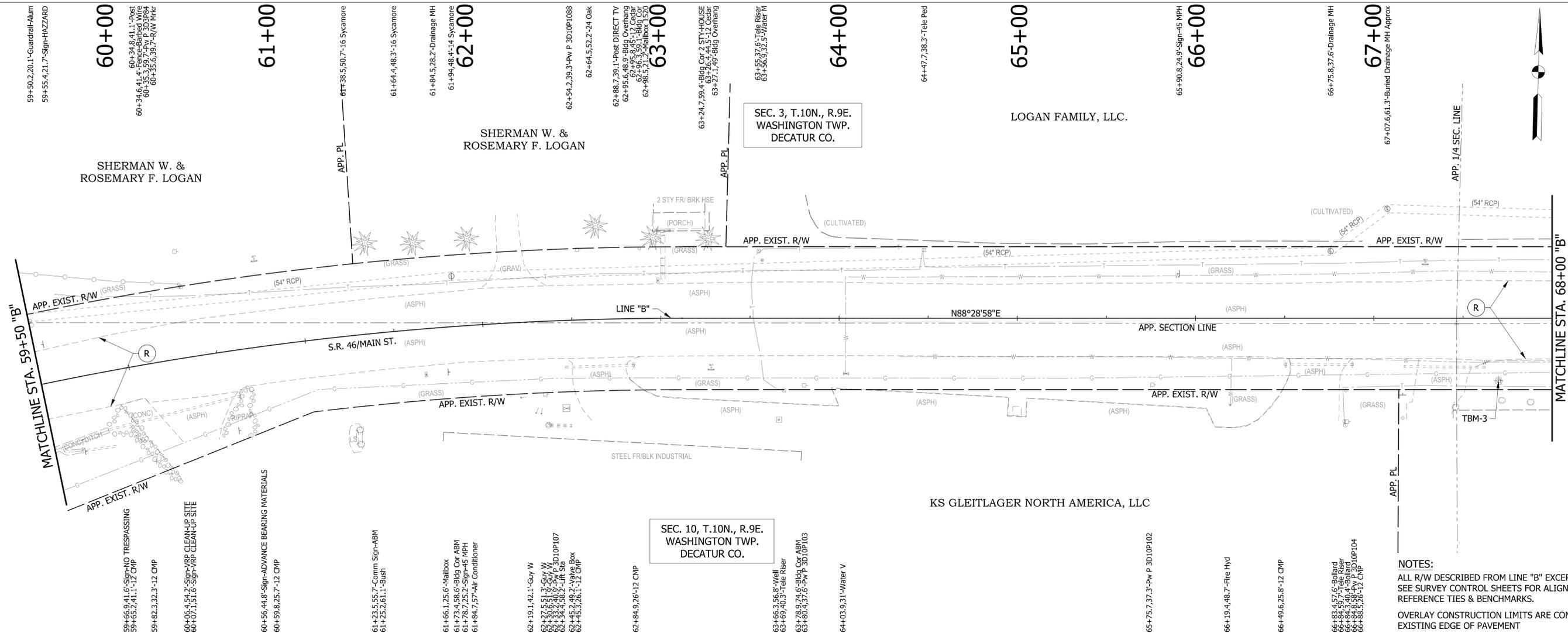
HORIZONTAL SCALE 1"=30'	BRIDGE FILE
VERTICAL SCALE 1"=10'	DESIGNATION 1800255
SURVEY BOOK	SHEETS
CONTRACT R-41463	39 of 124 PROJECT 1800256



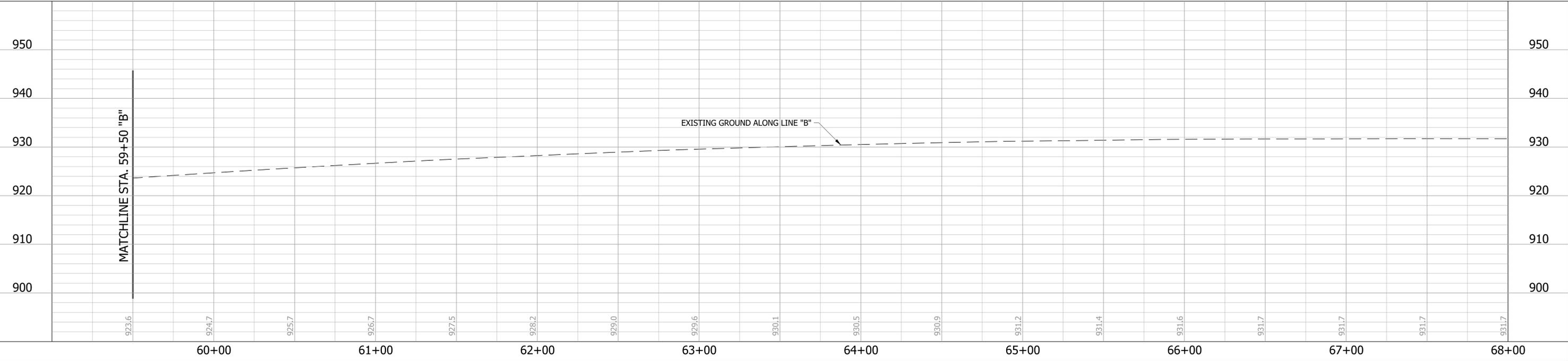
**NOTES:**  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.  
 OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
 EXISTING EDGE OF PAVEMENT



<b>LEGEND</b> (R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON, EXISTING, MILLED ASPHALT SURFACE	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>  <b>PLAN &amp; PROFILE LINE "B"</b>	HORIZONTAL SCALE 1"=30' VERTICAL SCALE 1"=10'	BRIDGE FILE _____ DESIGNATION 1800255
	DESIGNED: DC DRAWN: JEC CHECKED: LDW CHECKED: WRC		SURVEY BOOK _____ SHEETS 40 of 124 CONTRACT R-41463 PROJECT 1800256	



**NOTES:**  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.  
 OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
 EXISTING EDGE OF PAVEMENT



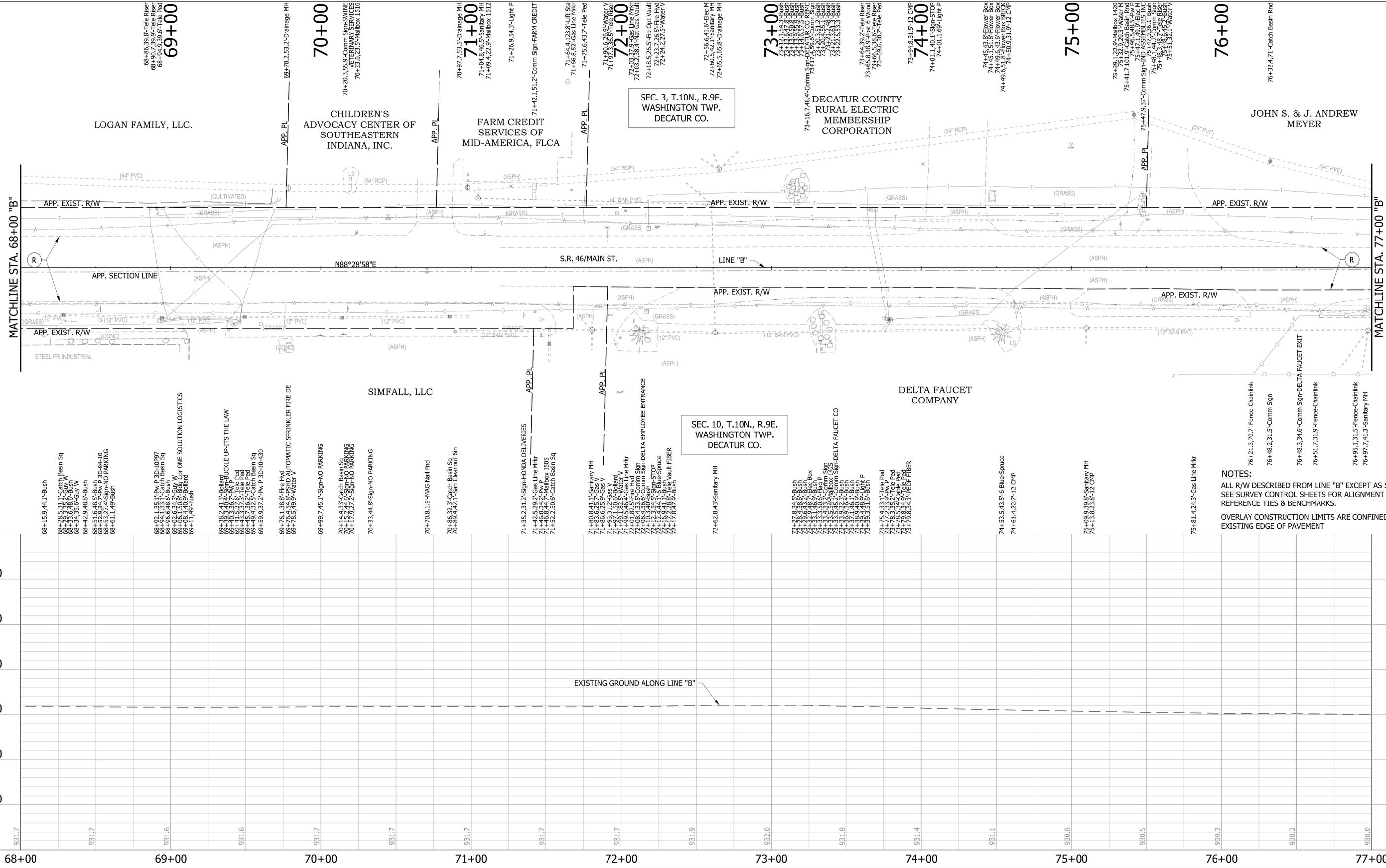
**LEGEND**  
 (R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON,  
 EXISTING, MILLED ASPHALT SURFACE

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE  
LINE "B"**

HORIZONTAL SCALE 1"=30'	BRIDGE FILE
VERTICAL SCALE 1"=10'	DESIGNATION 1800255
SURVEY BOOK	SHEETS
CONTRACT R-41463	41 of 124 PROJECT 1800256

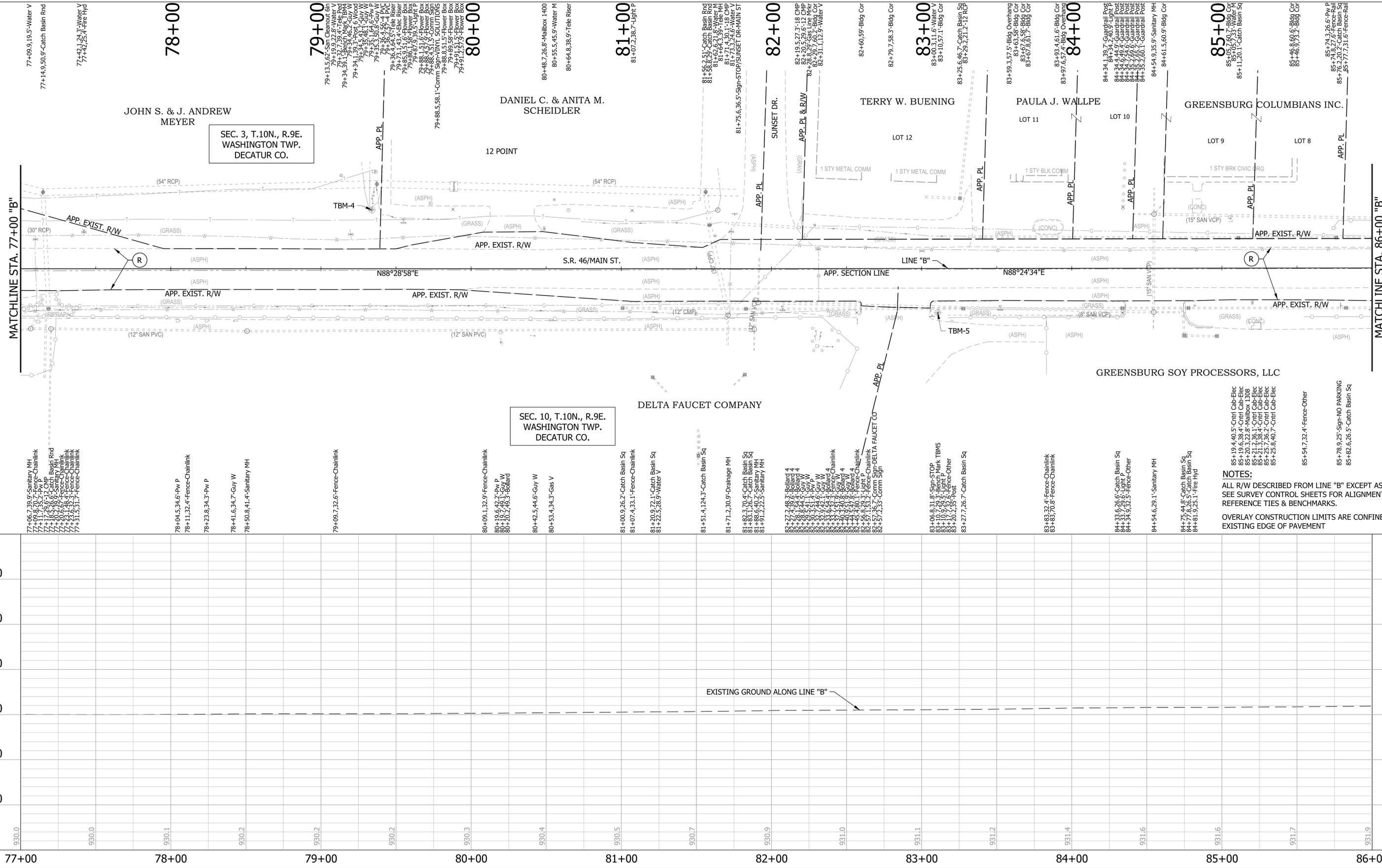


SEC. 3, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

NOTES:  
ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
REFERENCE TIES & BENCHMARKS.  
OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
EXISTING EDGE OF PAVEMENT

<b>LEGEND</b> 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON, EXISTING, MILLED ASPHALT SURFACE	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	<b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b>  <b>PLAN &amp; PROFILE</b> <b>LINE "B"</b>	HORIZONTAL SCALE 1"=30' VERTICAL SCALE 1"=10'	BRIDGE FILE DESIGNATION 1800255
	DESIGNED: DC CHECKED: LDW		DRAWN: JEC CHECKED: WRC	SURVEY BOOK CONTRACT R-41463



SEC. 3, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

SEC. 10, T.10N., R.9E.  
WASHINGTON TWP.  
DECATUR CO.

**NOTES:**  
ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
REFERENCE TIES & BENCHMARKS.  
  
OVERLAY CONSTRUCTION LIMITS ARE CONFINED TO THE  
EXISTING EDGE OF PAVEMENT

**LEGEND**  

 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON EXISTING, MILLED ASPHALT SURFACE

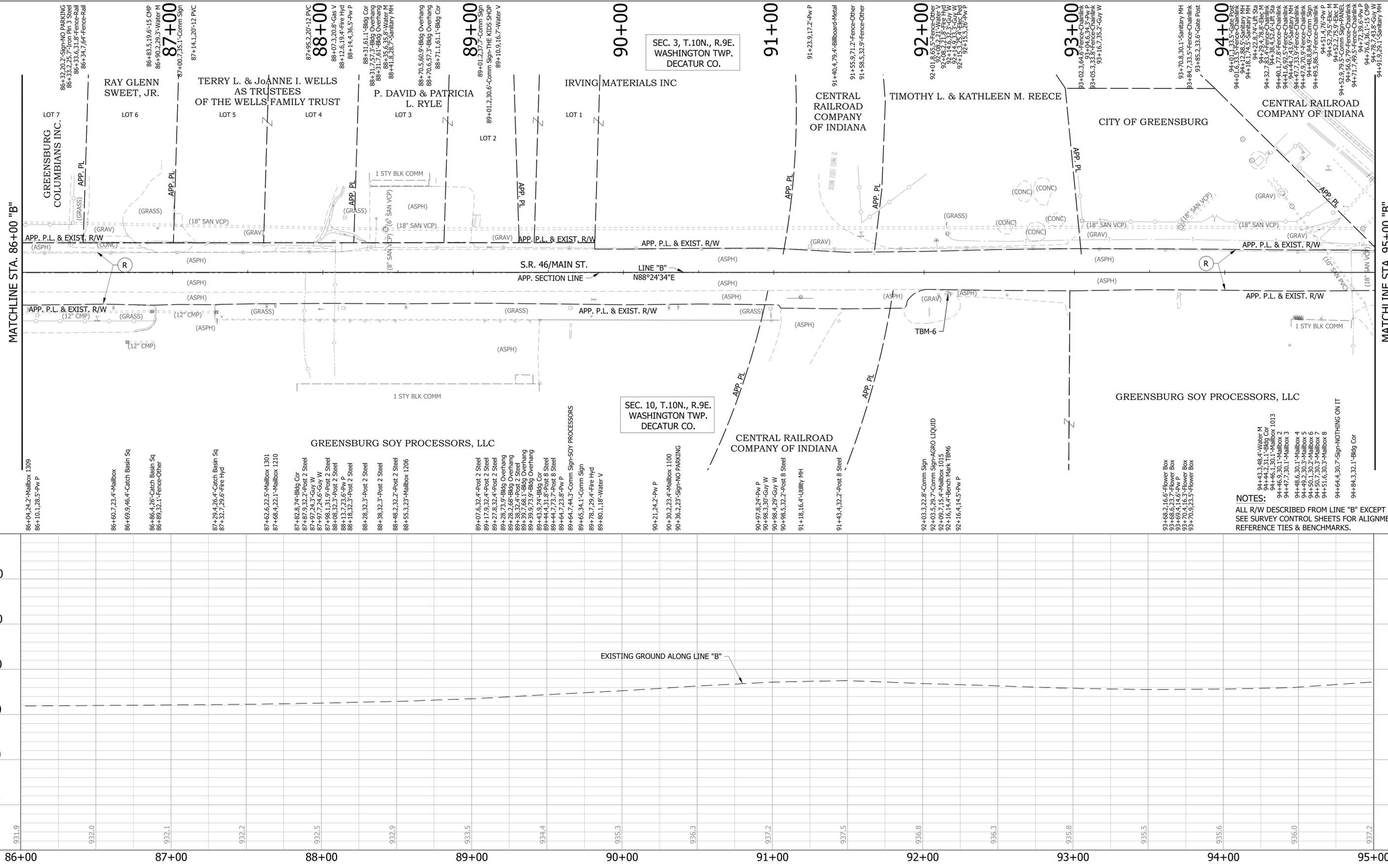
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

INDIANA  
DEPARTMENT OF TRANSPORTATION  
  
PLAN & PROFILE  
LINE "B"

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	43 of 124
CONTRACT	PROJECT
R-41463	1800256

MATCHLINE STA. 86+00 "B"

MATCHLINE STA. 95+00 "B"



SEC. 3, T.10N., R.9E. WASHINGTON TWP. DECATUR CO.

SEC. 10, T.10N., R.9E. WASHINGTON TWP. DECATUR CO.

NOTES:  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.

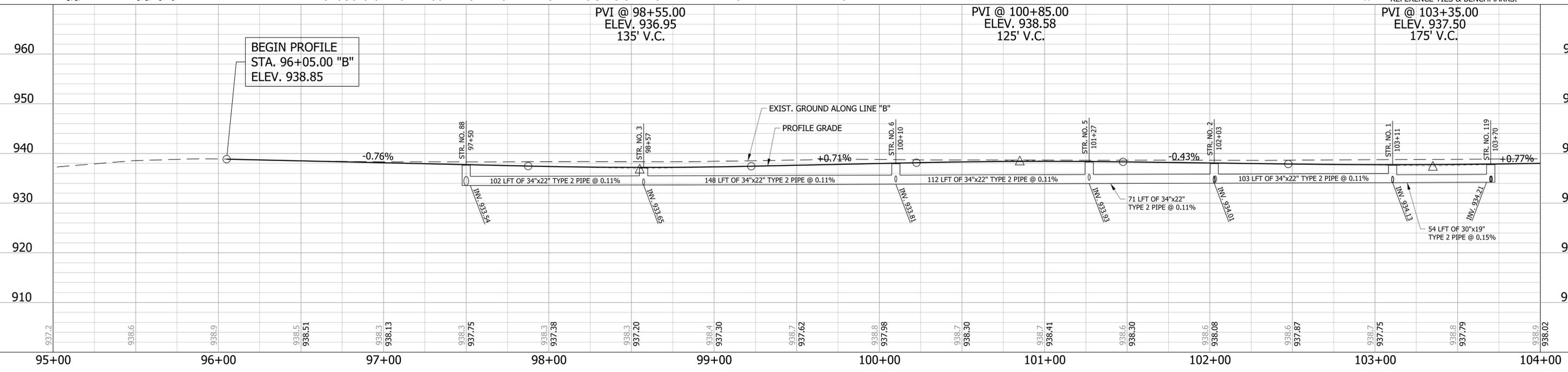
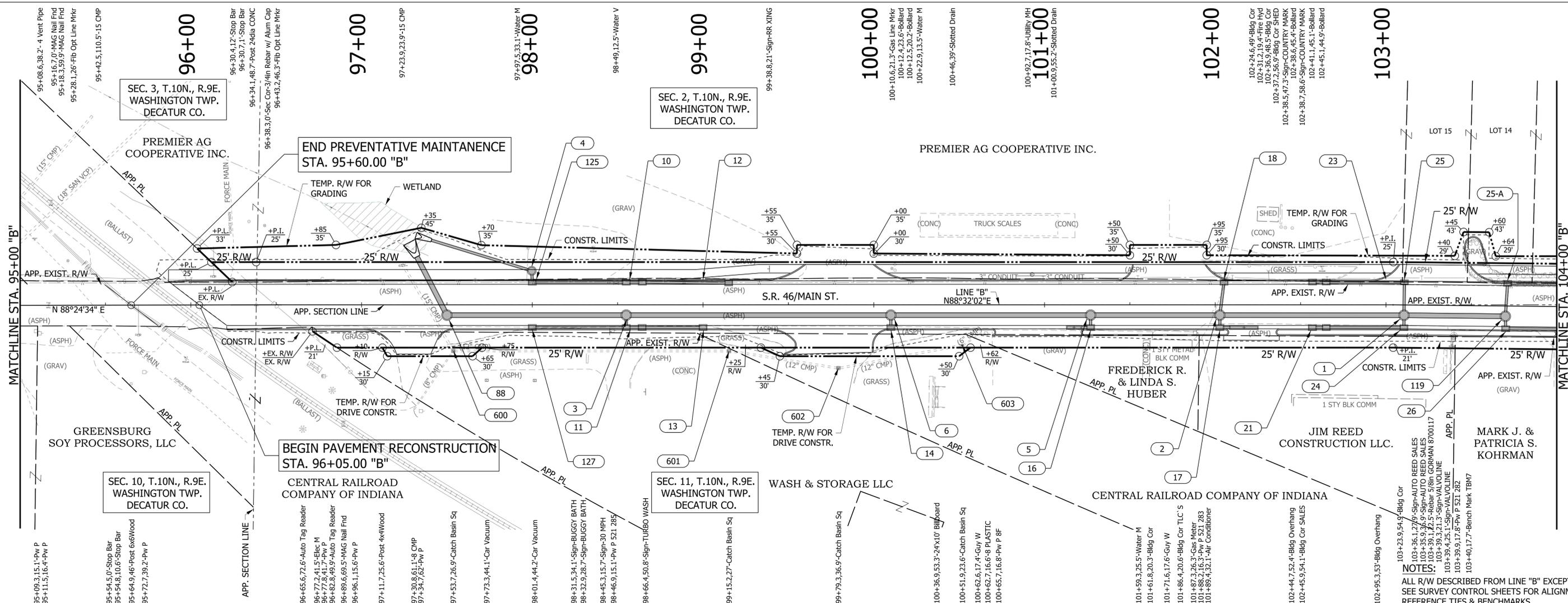
**LEGEND**  
 (R) 165 LB/SYS QC/QA-HMA, 3, 70, SURFACE 9.5 MM ON, EXISTING, MILLED ASPHALT SURFACE

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE**  
**LINE "B"**

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	44 of 124
CONTRACT	PROJECT
R-41463	1800256



**LEGEND**

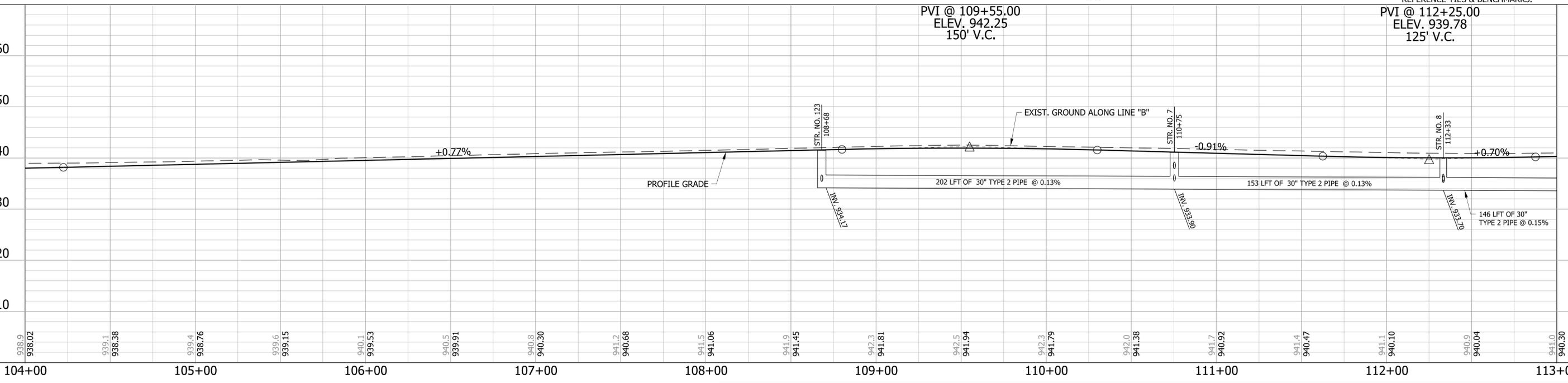
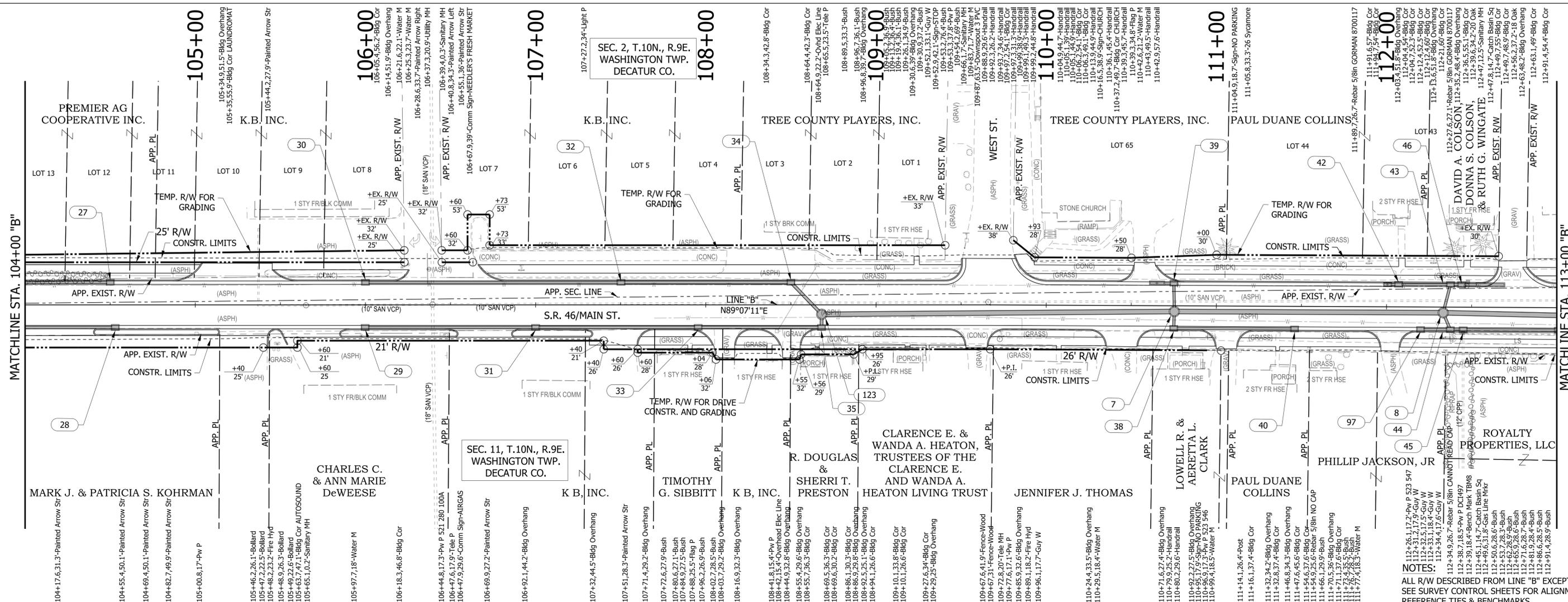
XXX STRUCTURE NUMBER - SEE CONSTRUCTION DETAILS FOR STRUCTURE NOTES

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DC	DRAWN: JEC	
CHECKED: LDW	CHECKED: WRC	

**INDIANA DEPARTMENT OF TRANSPORTATION**

**PLAN & PROFILE LINE "B"**

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	45 of 124
CONTRACT	PROJECT
R-41463	1800256



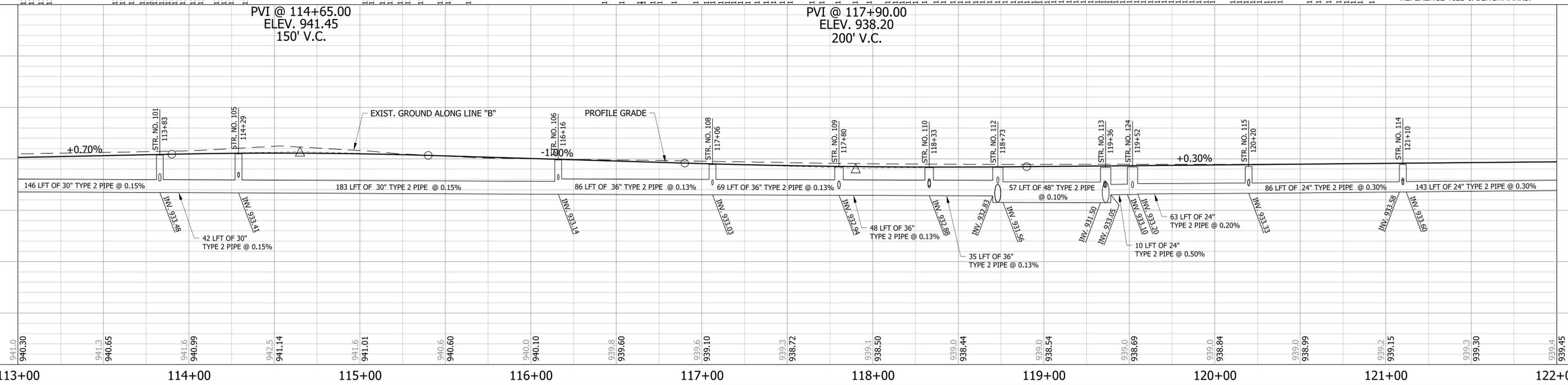
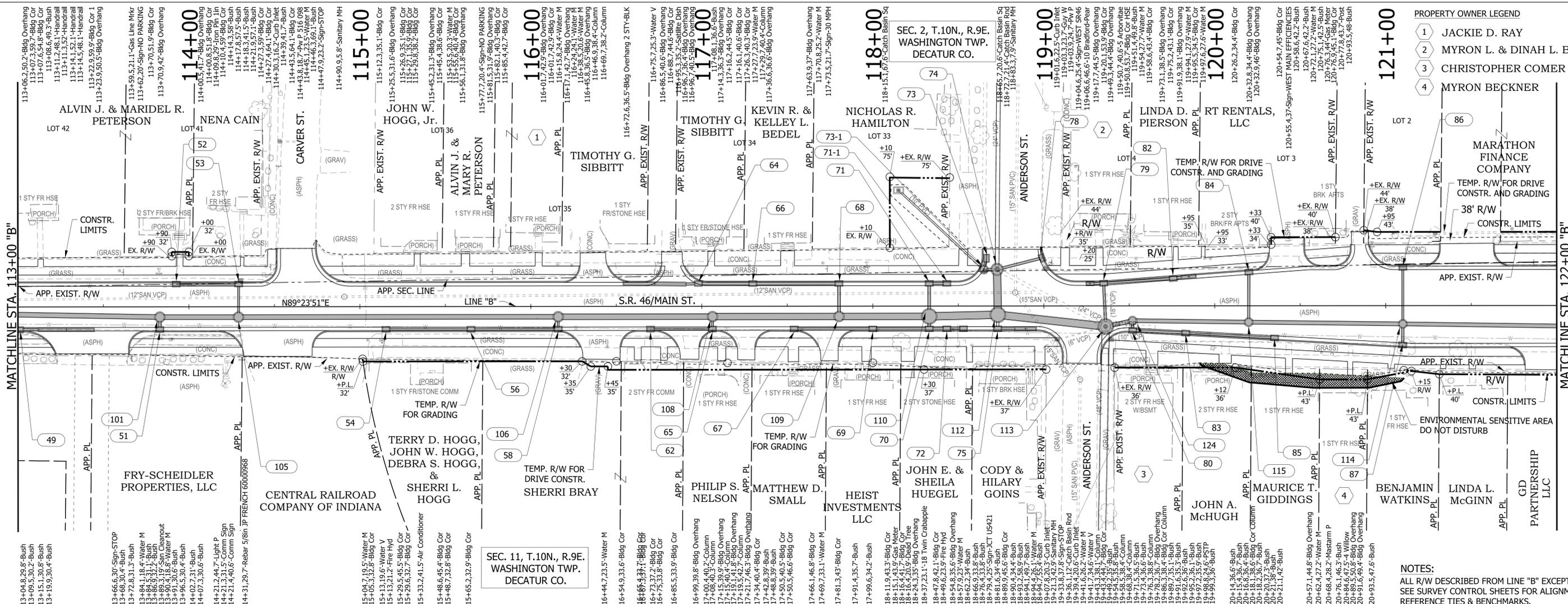
NOTES:  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.

LEGEND  
 XXX STRUCTURE NUMBER - SEE CONSTRUCTION DETAILS FOR STRUCTURE NOTES

RECOMMENDED FOR APPROVAL \_\_\_\_\_  
 DESIGN ENGINEER DATE  
 DESIGNED: DC DRAWN: JEC  
 CHECKED: LDW CHECKED: WRC

INDIANA  
 DEPARTMENT OF TRANSPORTATION  
 PLAN & PROFILE  
 LINE "B"

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	
VERTICAL SCALE	DESIGNATION
1"=10'	1800255
SURVEY BOOK	SHEETS
	46 of 124
CONTRACT	PROJECT
R-41463	1800256



**NOTES:**  
 ALL R/W DESCRIBED FROM LINE "B" EXCEPT AS SHOWN.  
 SEE SURVEY CONTROL SHEETS FOR ALIGNMENT DATA,  
 REFERENCE TIES & BENCHMARKS.

**PROPERTY OWNER LEGEND**

1	JACKIE D. RAY
2	MYRON L. & DINAH L. BECKNER
3	CHRISTOPHER COMER
4	MYRON BECKNER

<b>LEGEND</b> XXX STRUCTURE NUMBER - SEE CONSTRUCTION DETAILS FOR STRUCTURE NOTES	<b>INDIANA</b> <b>DEPARTMENT OF TRANSPORTATION</b>		HORIZONTAL SCALE 1"=30' VERTICAL SCALE 1"=10'	BRIDGE FILE DESIGNATION 1800255
	RECOMMENDED FOR APPROVAL DESIGN ENGINEER DATE	<b>PLAN &amp; PROFILE</b> <b>LINE "B"</b>		SURVEY BOOK CONTRACT R-41463
DESIGNED: DC CHECKED: LDW	DRAWN: JEC CHECKED: WRC			